

Building Survey & Photographic Record (Level 3)

Written Record (Volume 1)

DR017/Orion 5 Building,
Land at Curran Embankment, Cardiff

October 2023

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Executive Summary

1. This Building Survey and Photographic Record report ('the Building Record') relates to the DR017/Orion 5 Building, Land at Curran Embankment, Cardiff (National Grid Reference: ST18467508) ('the Site').
2. A planning application was submitted on 26th March 2021 (ref. no. 21/00783/MJR, reproduced at **Appendix 2**) for:

“Demolition of existing buildings/structures and the comprehensive mixed use redevelopment to provide up to 2,500 new homes (Class C3), business space (Class B1) and a mix of complementary leisure, food and drink, hospitality, retail and health and wellbeing uses (Class A1, A2, A3, C1, D1 and D2); creation of new open space (including a new riverside park and water taxi stop); new pedestrian, cycle and vehicular access points; pedestrian footbridge; vehicular and cycle parking facilities; landscaping; public realm and other associated ancillary and highways works. All matters reserved other than vehicular access.”

3. This Building Record has been agreed in correspondence with Glamorgan-Gwent Archaeological Trust ('GGAT') (dated 25th May 2021), who in response to that application, stated that one building within the Site had sufficient heritage value to warrant consideration in the planning process and made the following recommendation:

“[a] building has been identified (DR017 in the GGAT report and Orion 5 in the Orion report) which was part of the Curran Works, producing shell and cartridge casings. Whilst the structure has clearly undergone subsequent alterations, original features survive.

As such it is our recommendation that the structure be recorded prior to the commencement of the development.”

4. A Written Scheme of Investigation (WSI) was prepared by Turley Heritage (**Appendix 1**) and a draft shared with GGAT prior to completion of this Building Record. The Building Record and WSI has been informed by the feedback received from GGAT.
5. This report is consistent with the best practice guidance set out in 'Understanding Historic Buildings: A Guide to Good Recording Practice' (Historic England, 2016) and Paragraph 5.27 'Building Recording' of 'Technical Advice Note (TAN) 24: The Historic Environment (2017)'. The Building Survey and Photographic Record has been prepared to a Level 3 Record, based upon this Historic England guidance.
6. This Written Record (Volume 1) has been prepared with reference to historic maps, published and archival sources, and an on-site visual inspection.
7. The accompanying Photographic Record (Volume 2) includes external elevation photographs (taken 9th August 2023) of the building prior to any works commencing at the Site. Internal access and photography were not possible given that the building is in a poor structural condition/state of repair and contaminated with asbestos throughout; it was, therefore, not possible to secure safe access at this time. Images have been selected based on ensuring the best possible representation of the building.

8. The combined Building Survey and Photographic Survey provides a robust and proportionate record of the building as it exists at the time of survey, prior to any works commencing at the Site, associated with the relevant planning application.
9. In accordance with the Written Scheme of Investigation (**Appendix 1**), the Building Survey and Photographic Record will be deposited in the following locations, in the specified format:
 - One PDF version on a memory stick/flash drive will be submitted to the National Monuments Record held by the Royal Commission on Ancient and Historical Monuments of Wales (RCAHMW). Confirmation of receipt by the RCAHMW will be provided to the Council to discharge the condition.
 - One PDF version, following approval by an archaeological advisor, will be submitted to The Glamorgan-Gwent Archaeological Trust (GGAT), with a summary description in both Welsh and English, and photographs as individual JPEG files with accompanying metadata.

Crynodeb Gweithredol

1. Mae'r adroddiad Arolwg Adeilad a Chofnod Ffotograffig hwn ('y Cofnod Adeilad') yn ymwneud ag Adeilad DR017/Orion 5, Tir yn Arglawdd Curran, Caerdydd (Cyfeirnod Grid Cenedlaethol: ST18467508) ('y Safle').
2. Cyflwynwyd cais cynllunio ar 26 Mawrth 2021 (cyfeirnod 21/00783/MJR, sydd wedi'i atgynhyrchu yn **Atodiad 2**) ar gyfer:

"Dymchwel adeiladau/strwythurau presennol a'r ailddatblygiad defnydd cymysg cynhwysfawr i ddarparu hyd at 2,500 o gartrefi newydd (Dosbarth C3), gofod busnes (Dosbarth B1) a chymysgedd o ddefnyddiau hamdden, bwyd a diod, lletygarwch, manwerthu ac iechyd a lles (Dosbarth A1, A2, A3, C1, D1 a D2); creu man agored newydd (gan gynnwys parc newydd ar lan yr afon a man aros ar gyfer tacsis dŵr); manau mynediad newydd i gerddwyr, beicwyr a cherbydau; pont droed i gerddwyr; cyfleusterau parcio i gerbydau a beiciau; tirlunio; tir y cyhoedd a gwaith priffyrdd ac ategol cysylltiedig arall. Cedwir pob mater yn ôl ac eithrio mynediad i gerbydau."

3. Cytunwyd ar y Cofnod Adeilad hwn mewn gohebiaeth ag Ymddiriedolaeth Archeolegol Morgannwg-Gwent (dyddiedig 25 Mai 2021), a ddywedodd mewn ymateb i'r cais hwnnw, fod gan un adeilad ar y Safle werth treftadaeth digonol i haeddu ystyriaeth yn y broses cynllunio a gwnaeth yr argymhelliad canlynol:

"mae adeilad wedi ei nodi (DR017 yn yr adroddiad Ymddiriedolaeth Archeolegol Morgannwg-Gwent ac Orion 5 yn adroddiad Orion) a oedd yn rhan o Waith Curran, yn cynhyrchu casynau cetrus a chregyn. Er bod y strwythur yn amlwg wedi cael ei addasu wedyn, mae nodweddion gwreiddiol wedi goroesi.

O'r herwydd, ein hargymhelliad yw y dylid cofnodi'r strwythur cyn bwrw ymlaen â'r datblygiad."

4. Paratowyd Cynllun Ymchwilio Ysgrifenedig gan Turley Heritage (**Atodiad 1**) a rhannwyd drafft gydag Ymddiriedolaeth Archeolegol Morgannwg-Gwent cyn cwblhau'r Cofnod Adeilad hwn. Mae'r Cofnod Adeilad a'r Cynllun Ymchwilio Ysgrifenedig wedi cael eu llywio gan yr adborth a gafwyd gan Ymddiriedolaeth Archeolegol Morgannwg-Gwent.
5. Mae'r adroddiad hwn yn cyd-fynd â'r canllawiau arfer gorau a nodir yn Understanding Historic Buildings: A Guide to Good Recording Practice' (Historic England, 2016) a Pharagraff 5.27 'Cofnodi Adeiladau' o 'Nodyn Cyngor Technegol 24: Yr Amgylchedd Hanesyddol (2017)'. Mae'r Arolwg Adeiladu a'r Cofnod Ffotograffig wedi cael eu paratoi ar gyfer Cofnod Lefel 3, yn seiliedig ar ganllawiau Historic England.
6. Mae'r Cofnod Ysgrifenedig hwn (Cyfrol 1) wedi cael ei baratoi gan gyfeirio at fapiau hanesyddol, ffynonellau archifol ac wedi'u cyhoeddi, ac archwiliad gweledol ar y safle.
7. Mae'r Cofnod Ffotograffig (Cyfrol 2) cysylltiedig yn cynnwys ffotograffau o'r adeilad (a dynnwyd ar 9 Awst 2023) o'r adeilad cyn i unrhyw waith ddechrau ar y Safle. Nid oedd ffotograffiaeth a mynediad y tu mewn yn bosibl o ystyried bod yr adeilad mewn cyflwr strwythurol gwael a'i fod wedi'i halogi ag asbestos drwyddi draw; felly, nid oedd yn

bosibl sicrhau mynediad diogel ar y pryd. Mae lluniau wedi cael eu dewis ar sail sicrhau bod yr adeilad yn cael ei gynrychioli yn y ffordd orau bosibl.

8. Mae'r Arolwg Adeiladu cyfun a'r Arolwg Ffotograffig yn darparu cofnod cadarn a chymesur o'r adeilad fel y mae ar adeg yr arolwg, cyn i unrhyw waith ddechrau ar y Safle, sy'n gysylltiedig â'r cais cynllunio perthnasol.
9. Yn unol â'r Cynllun Ymchwilio Ysgrifenedig (**Atodiad 1**), bydd yr Arolwg Adeiladu a'r Cofnod Ffotograffig yn cael eu cadw yn y lleoliadau canlynol, yn y fformat penodedig:
 - Bydd un fersiwn PDF ar gof bach/gyriant fflach yn cael ei gyflwyno i'r Cofnod Henebion Cenedlaethol a gedwir gan Gomisiwn Brenhinol Henebion Cymru. Bydd cadarnhad bod Comisiwn Brenhinol Henebion Cymru wedi'i gael yn cael ei roi i'r Cyngor i gyflawni'r amod.
 - Bydd un fersiwn PDF, yn dilyn cymeradwyaeth cynghorydd archaeolegol, yn cael ei gyflwyno i Ymddiriedolaeth Archaeolegol Morgannwg-Gwent, gyda disgrifiad cryno yn Gymraeg ac yn Saesneg, a ffotograffau fel ffeiliau JPEG unigol gyda metadata cysylltiedig.

Table 1.1: Key Information required for historic asset records¹

HER Field Name	Field Definition
Historic Asset PRN	05939s
Event PRN	E007524
Site Name English	Industrial Building, Curran Embankment, Cardiff
Site Name Welsh	Adeilad Diwydiannol, Arglawdd Curran, Caerdydd
Summary English	A large linear two storey red-brick building, at the Curran Embankment, Cardiff. A surviving industrial unit associated with the Curran Engineering Works, built in c.1942 as a munitions factory during the Second World War and later used for engineering and industrial products. The structure is in a disused and poor condition, stripped out/derelect inside. It has undergone significant alteration, including the loss of many original windows, insertion of vehicular openings, and modification to the roof, though with original external walls remaining largely intact.
Summary Welsh	Adeilad brics coch deulawr llinol mawr, yn Arglawdd Curran, Caerdydd. Uned ddiwydiannol sydd wedi goroesi sy'n gysylltiedig â Gwaith Peirianeg Curran, a adeiladwyd tua 1942 fel ffatri arfau yn ystod yr Ail Ryfel Byd ac a ddefnyddiwyd wedyn ar gyfer cynhyrchion peirianeg a diwydiannol. Nid yw'r strwythur yn cael ei ddefnyddio ac mae ei gyflwr yn wael, mae wedi cael ei wagio/esgeuluso. Mae wedi newid yn sylweddol, gan gynnwys colli llawer o'r ffenestri gwreiddiol, mewnosod agoriadau i gerbydau, ac addasu'r to, er bod y waliau allanol gwreiddiol yn dal yn gyfan i raddau helaeth.
Description	<p>The 1940s building follows a two-storey linear plan form, extending over approximately 340m at its widest point. It comprises two long bays with pitched roofs supported by trusses, which would originally have been portioned and subdivided within, separating the specific functions of individual areas. Additional smaller single storey bays extend from the centre of the north elevation and the west side of the south elevation. Notable later extensions and adaptations to the original building are evident in the raised corrugated iron roof and shed structures attached at the building's western end, as well as modern steel framed vehicle entrances inserted regularly along the north elevations.</p> <p>Outer walls of the original structure are predominantly constructed of red brick with corrugated iron roofs supported by metal trusses. Walls have been painted over in blue and white over a select area at the western end of the building's south elevation. Later corrugated iron extensions are also evident.</p> <p>The north elevation includes a formal tripartite entrance with white painted concrete architraves at its eastern end, otherwise continuing with regularly spaced metal framed multi-paned windows, of which many have been covered over or filled in with breeze blocks, with loss to glazing. Modern vehicle entrances with steel frames and shutters have been inserted at various points, interrupting the original architectural composition.</p>

¹ Derived from Figure 2, Guidance for the Submission of Data to the Welsh Historic Environment Records (HERs) The Welsh Archaeological Trusts, October 2022, pp. 13-14

Table 1.1: Key Information required for historic asset records¹

HER Field Name	Field Definition
	<p>The west and east sides of the building are marked by angular structures addressing the riverbank and Curran Road respectively, of an unusual profile in relation the rest of the building.</p> <p>The south elevation continues with the same material palate as found elsewhere, with similar fenestration broken up by modern additions and smaller pedestrian entrances, set at different parts of the building with concrete lintels. A single ramp and raised vehicle entrance are positioned at the east end. Iron railings remain over select lengths of the outer roofline, though have been removed in most places.</p> <p>The structure is of standard manufacture and of limited architectural or aesthetic value, being a comparatively plain, functional structure. Building materials used are prosaic and common to this period, with many parts the original fabric altered, damaged, or removed. The building has some illustrative/evidential value in connection with its function during the Second World War, though this has been eroded substantially due to the loss of original fabric and fixtures, particularly within the interior. Remaining sections of track, formerly used for transportation of products and materials and preserved within the road surface adjacent, show a very small level of illustrative value.</p> <p>The building has historic interest as part of a key wartime phase in the development of the Curran Embankment, when from 1942 a new 100,000 square foot munitions complex was built. The building forms the only remaining part of this complex. Additionally, the building has some interest in connection with its post-Second World War uses, having been adapted by Curran Steels Ltd for the assembly of a variety of engineering/industrial products in the 1950s. An appreciation of this historic interest of the building has been significantly lessened, owing to the building's poor condition, reduced state of intactness, and modern context.</p> <p>Historically, the building formed part of a wider 1940s industrial and munitions complex extending over a much wider area at the Curran Embankment. The building is now the last remaining element of a series of contemporaneous former warehouses and is now surrounded by later development, including modern warehouse structures. The building is contained to the far south of the Curran Embankment site, with the linear warehouse to the north similar in its proportions, with angularly composed elevations to the east addressing the Curran Road of a like shape, indicating it was built to the footprint and scale of an early 1940s warehouse. 20th century OS Maps also confirm this. There is nothing preserved within the building's setting, aside from a few isolated and fragmentary historic features, indicative of the former 1940s munitions infrastructure. No group value can be assigned to the building in relation to the industrial context or the wider townscape.</p>
NGR	ST18467508
Easting	318460

Table 1.1: Key Information required for historic asset records¹

HER Field Name	Field Definition
Northing	175080
NGR Qualifier	Approximate
NGR Derivation	National Grid Reference Source
Historic Asset Type	Industrial Building
Event Type	Building Recording
Period	20 th Century
Survival Condition	Converted or Re-used; Damaged
Condition Rating	Poor
Condition Description	In a disused and poor condition, stripped out/derelict inside, with loss to many original windows, removal of external wall fabric via insertion of later vehicular openings, modification to the roof with a raised corrugated metal section to the west. Original external walls and roof sections remaining largely intact.
Broadclass	Physical Evidence
Evidence	Building
Year	2023
References	<p>Davies, J. 1981 ,Cardiff and the Marquesses of Bute. Cardiff: University of Wales Press</p> <p>Foreign Commerce Weekly, 18 May 1959, Company in Wales Offers Facilities</p> <p>Google Earth Historical Imagery</p> <p>Mortimer, D. , 2014 ,Cardiff: The Biography Ordnance Survey ,</p> <p>1st Edition OS map 1:2500 Ordnance Survey ,</p> <p>2nd Edition OS map 1:2500 Ordnance Survey ,</p> <p>3rd Edition OS map 1:2500 Ordnance Survey ,</p> <p>4th Edition OS map 1:2500</p> <p>Roberts R. , 2014 , Dumballs Road, Cardiff: archaeological desk-based assessment</p> <p>Roberts, R. , 2015 , Engineering works at Dumballs in Cardiff</p>

Table 1.1: Key Information required for historic asset records¹

HER Field Name	Field Definition
Record Compiled By	Harry Best-Shaw, MA (Hons), MSc
Record Compiled By	Turley Heritage
Record Compiled On	30/10/23

1. Introduction

- 1.1 This Building Survey and Photographic Record report ('the Building Record') relates to a building within the former Curran Engineering Works site (identified as the DR017/Orion 5 Building) on the Land at the Curran Embankment, Cardiff. (National Grid Reference: ST18467508) ('the Site') (as shown at **Figure 1.1**).

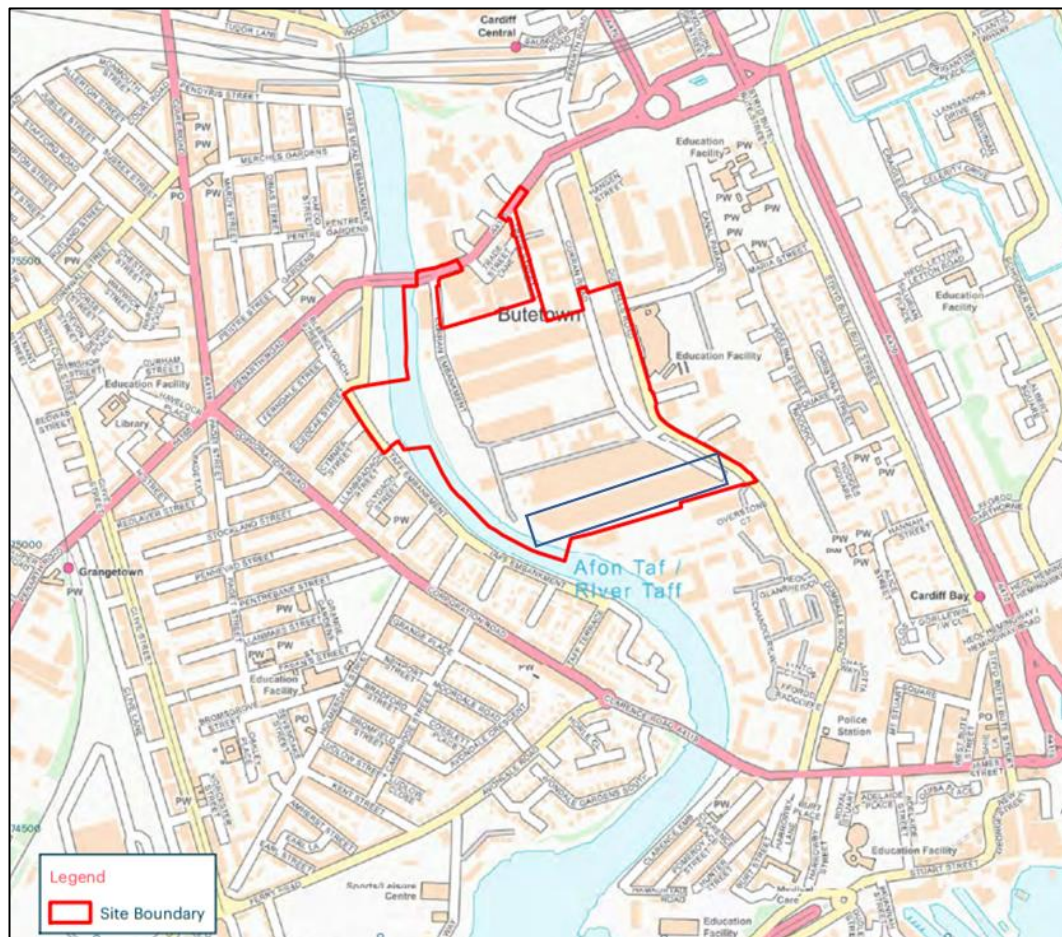


Figure 1.1: Site plan (DR017/Orion 5 building marked in blue)

Background

- 1.2 This Building Record and Photographic Survey has been prepared by Harry Best-Shaw MA(Hons), MSc, Consultant at Turley Heritage, with oversight from Victoria Bellamy, Senior Consultant, and Marc Timlin, Director, Head of Heritage, Townscape and Landscape.
- 1.3 It relates to the planning permission (ref no. 21/00783/MJR) reproduced at **Appendix 2**) received by Cardiff City Council in March 2021 for the following scheme at Land at the Curran Embankment, Cardiff:

“Demolition of existing buildings/structures and the comprehensive mixed use redevelopment to provide up to 2,500 new homes (Class C3), business space (Class B1)

and a mix of complementary leisure, food and drink, hospitality, retail and health and wellbeing uses (Class A1, A2, A3, C1, D1 and D2); creation of new open space (including a new riverside park and water taxi stop); new pedestrian, cycle and vehicular access points; pedestrian footbridge; vehicular and cycle parking facilities; landscaping; public realm and other associated ancillary and highways works. All matters reserved other than vehicular access.”

- 1.4 This process has been agreed in correspondence with GGAT (dated 25th May 2021), who in response to this application, stated that the proposal had an archaeological restraint such that:

“[a] building has been identified (DR017 in the GGAT report and Orion 5 in the Orion report) which was part of the Curran Works, producing shell and cartridge casings. Whilst the structure has clearly undergone subsequent alterations, original features survive.

As such it is our recommendation that the structure be recorded prior to the commencement of the development.”

- 1.5 The DR017/Orion 5 Building dates from the 1940s and extends across the south of the Site, comprising a long two-storey warehouse structure extending between the River Taff and Curran Road.

- 1.6 A Written Scheme of Investigation (WSI) was prepared (Turley Heritage, November 2022) (**Appendix 1**) and a draft shared informally with GGAT prior to the completion of this Building Record.

- 1.7 In preparing this Building Record, regard has been had to the guidance set out within Planning Policy Wales (Edition 11, February 2021), and Technical Advice Note (TAN) 24: The Historic Environment (2017), as the relevant policy and guidance basis for undertaking a historic building survey and photographic record in Wales. These policies specifically relate to the recording of listed buildings but can be used to give an indication of the correct approach for undertaking survey or photographic recording in other circumstances.

- 1.8 Paragraph 6.1.27 of ‘Planning Policy Wales (Edition 11)’ states that:

“If the planning authority is minded to approve an application and where archaeological remains are affected by proposals that alter or destroy them, the planning authority must be satisfied that the developer has secured appropriate and satisfactory provision for their recording and investigation, followed by the analysis and publication of the results and the deposition of the resulting archive in an approved repository. On occasions, unforeseen archaeological remains may still be discovered during the course of a development. A written scheme of investigation should consider how to react to such circumstances, or it can be covered through an appropriate condition for a watching brief. Where remains discovered are deemed to be of national importance, the Welsh Ministers have the power to schedule the site and in such circumstances scheduled monument consent must be required before works can continue.”

- 1.9 Paragraph 5.27 ‘Building Recording’ of ‘Technical Advice Note (TAN) 24: The Historic Environment (2017)’ states:

“Works undertaken as the result of a listed building consent will often present the opportunity to make a record of features of the building that are to be removed or altered, or which may be revealed during the course of works. Works associated with a listed building consent may also reveal archaeological remains of earlier phases of the building or items that are associated with the building. When considering a listed building consent application, the local planning authority must decide whether a programme of building recording and/or archaeological investigation is required to record the features that might be revealed or destroyed. They may seek the advice of their archaeological advisors and impose a suitable condition relating to recording (see 4.13). Applicants should contact the local planning authority when unexpected discoveries are made during the course of works. The Royal Commission on the Ancient and Historical Monuments of Wales must be given an opportunity to record a listed building proposed for demolition.”

1.10 The DR017/Orion 5 Building is neither a designated nor a formally identified locally historic asset in the terms of Planning Policy Wales.

1.11 Further specific guidance has also been referred to, however, in lieu of any specific Welsh guidance on the production of recording reports (i.e., by Cadw), the content and structure of the proposed recording report is drawn from Historic England’s best practice guidance:

- *Understanding Historic Buildings: A guide to good recording practice* (Historic England, May 2016);
- *Analysis and Recording for the Conservation and Control of Works to Historic Buildings* (Association of Local Government Archaeological Officers, 1997);
- *Understanding Historic Buildings: Policy and Guidance for Local Authorities* (Historic Environment Local Management, 2008);
- *Standard and guidance for the archaeological investigation and recording of standing buildings or structures* (Chartered Institute for Archaeologists, 2020)
- *Standard and guidance for the creation, compilation, transfer and deposition of archaeological archives* (Chartered Institute for Archaeologists, 2020)
- *Guidance for the Submission of Data to the Welsh Historic Environment Records (HERs)* (The Welsh Archaeological Trusts, October 2022); and
- *Guidelines for Digital Archaeological Archives* (RCAHMW, March 2015)

1.12 The Written Account (Volume 1) and Photographic Record (Volume 2) have been prepared to Level 3, in accordance with the agreed Written Scheme of Investigation (**Appendix 1**).

- This Written Account (Volume 1) contains the following information:
 - The precise location of the building as an address and in the form of a National Grid reference;

- A note of any statutory designation i.e. listing, scheduling, Register of Historic Parks and Gardens, conservation area;
 - The date when the record was made, the name(s) of the recorder(s) and the location of any archive material;
 - A summary statement of the building’s form, function, date and sequence of development, providing the names of patrons and owners and a description of the building;
 - An introduction briefly setting out the circumstances in which the record was made, its objectives, methods, scope and limitations, and any constraints;
 - Acknowledgements to all those who have made a significant contribution to the making of the record;
 - A discussion of the published sources relating to the building and its setting, including an account of its history as given in published sources, an analysis of historic map evidence (map regression);
 - An account of the building’s overall form (structure, materials, layout) and of its successive phases of development, together with the evidence supporting this analysis;
 - An account of the building’s past and present use;
 - Existing survey drawings (**Appendix 4**);
 - Full bibliographic list of the sources consulted (**Appendix 5**).
- The Photographic Record (Volume 2) comprises:
 - General photographs of all accessible areas;
 - The relationship of the building to its context; and
 - A record of relevant architectural details².

1.13 This Written Record (Volume 1) has been prepared with reference to historic maps, published and archival sources, and an on-site visual inspection.

1.14 The Photographic Record (Volume 2) includes external elevation photographs (taken 9th August 2023) of the building prior to any works commencing at the Site. Internal access for photography was not possible given the high safety risk; the building is in a poor structural condition/state of repair and contaminated with asbestos throughout. Images have been selected based on ensuring the best possible representation of the building.

² In those instances where the same detail is reproduced in more than one location, a representative example was captured as part of the photographic survey

- 1.15 The combined Building Survey and Photographic Survey provides a robust and proportionate record of the building as it exists at the time of survey, prior to any works commencing at the Site, associated with the relevant planning application.
- 1.16 The Heritage Statement prepared by Orion Heritage, which accompanied the application for planning permission (**Appendix 3**), provides a robust and proportionate assessment of the historic development of the Site and the significance of the subject building. This work informs this Building Record where appropriate.

Limitations / Constraints

- 1.17 The survey is based on a visual inspection of the readily accessible areas of the Site and its local context.
- 1.18 Within the Site, the confined nature of some areas required a mix of photography and various focal lengths to capture a reasonable record.

Location of Record

- 1.19 In accordance with the agreed Written Scheme of Investigation (**Appendix 1**), the Building Survey and Photographic Record will be deposited in the following locations, in the specified format:
- One PDF version on a memory stick/flash drive will be submitted to the National Monuments Record held by the Royal Commission on Ancient and Historical Monuments of Wales (RCAHMW). Confirmation of receipt by the RCAHMW will be provided to the Council to discharge the condition.
 - One PDF version, following approval by an archaeological advisor, will be submitted to The Glamorgan-Gwent Archaeological Trust (GGAT), with a summary description in both Welsh and English, and photographs as individual JPEG files with accompanying metadata.

2. Written Account

Introduction

- 2.1 The building is not a designated or formally recognised non-designated historic asset and is not located within the boundaries of a large historic asset or setting of any historic asset.
- 2.2 In considering the architectural and historic interest of the building, reference will be made to TAN24; Cadw, Conservation Principles for the Sustainable Management of the historic environment on Wales (2011); and Historic England's 'Statements of Heritage Significance: Analysis Significance in Heritage Assets (2019)'.

Drawings

- 2.3 The applicant has prepared a measured set of existing elevations, plans and sections of the DR017/Orion 5 Building. These drawings are included as **Appendix 4** to this report.

Building Assessment

- 2.4 This section of Building Record includes an overview of the former use, history and development of the building and provides dates and phasing where possible. This is followed by a description of the building and an outline of its significance.
- 2.5 The following building assessment is informed by the March 2021 Orion Heritage Statement (**Appendix 3**), which accompanied the applications for planning permission (ref no. 21/00783/MJR).
- 2.6 Assessment is based on a review existing published information and on-site visual survey and analysis.

Historical Development of the Site

- 2.7 In 1878, Glamorganshire Canal Navigation applied for a bill to authorise the construction of a docks, basins, quays, jetties, wharves, and warehouses on the east side of the Glamorganshire Canal and on the west side between the canal and the River Taff. Between 1880 and 1900, a second much larger pond was excavated on the west side of Dumballs Road (**Figure 2.1**) where timber was stored in water to prevent it seasoning before being processed at the neighbouring sawmill. This larger pond was filled in during the late 1930s to make way for a series of factories and engineering works, each furnished with private sidings connected to the Cardiff Riverside Branch of the Great Western Railway.

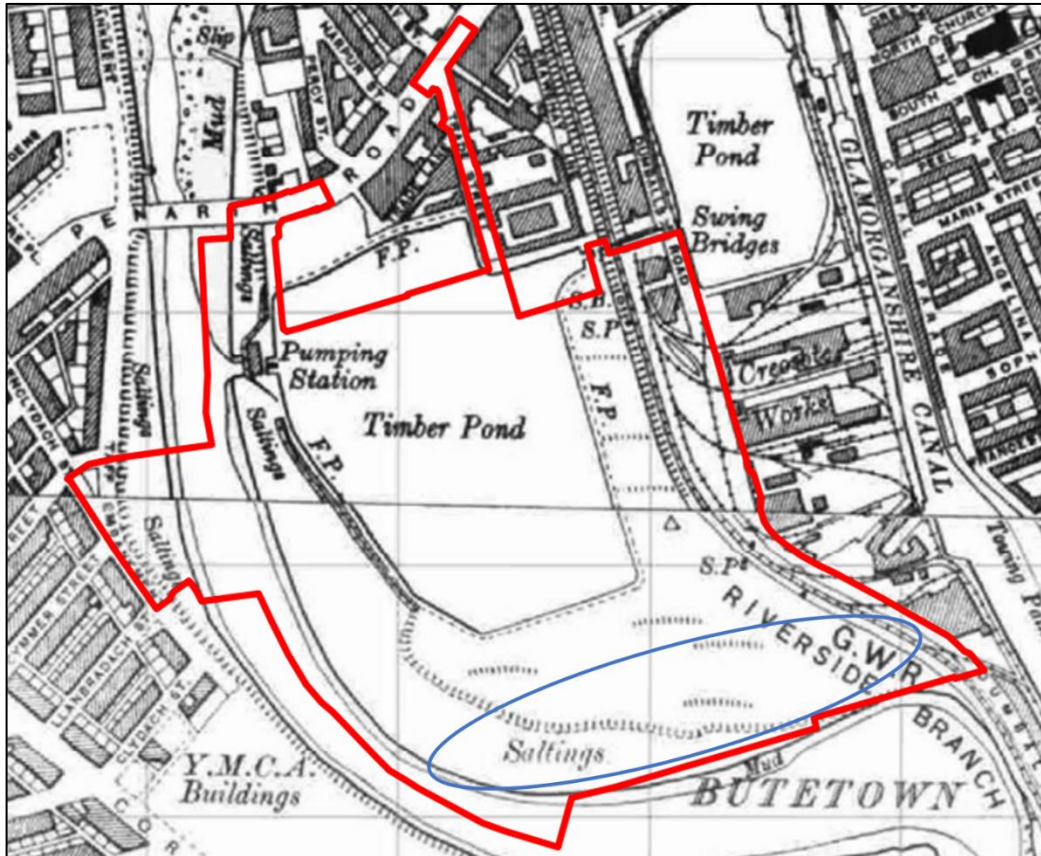


Figure 2.1: 1915-16 Ordnance Survey extract, 1:10,560 Scale Map (Site boundary red, DR017/Orion 5 Building blue)

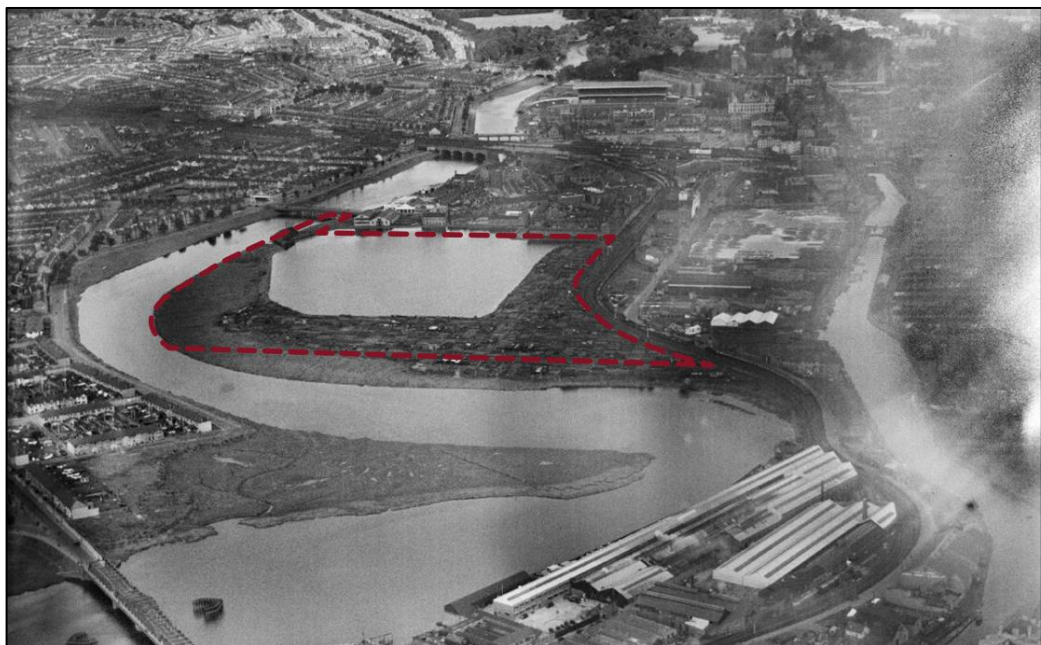


Figure 2.2: 1937 pre-war aerial photograph of the undeveloped Site at the Curran Embankment

2.8 Probably the best-known occupant of the site was Curran Steels. Curran produced munitions in the 1930s, as part of Britain's preparations for the Second World War.³ The new works at Curran Road were opened in 1942 (of which the subject building formed a part (**Figure 2.2**) and included a 100,000 square foot steel foundry with a laboratory, dressing and pattern shops. From the 1950s, Curran Steels Ltd. manufactured earthmoving equipment, industrial presses, castings, and general engineering products at the works on Curran Road.⁴

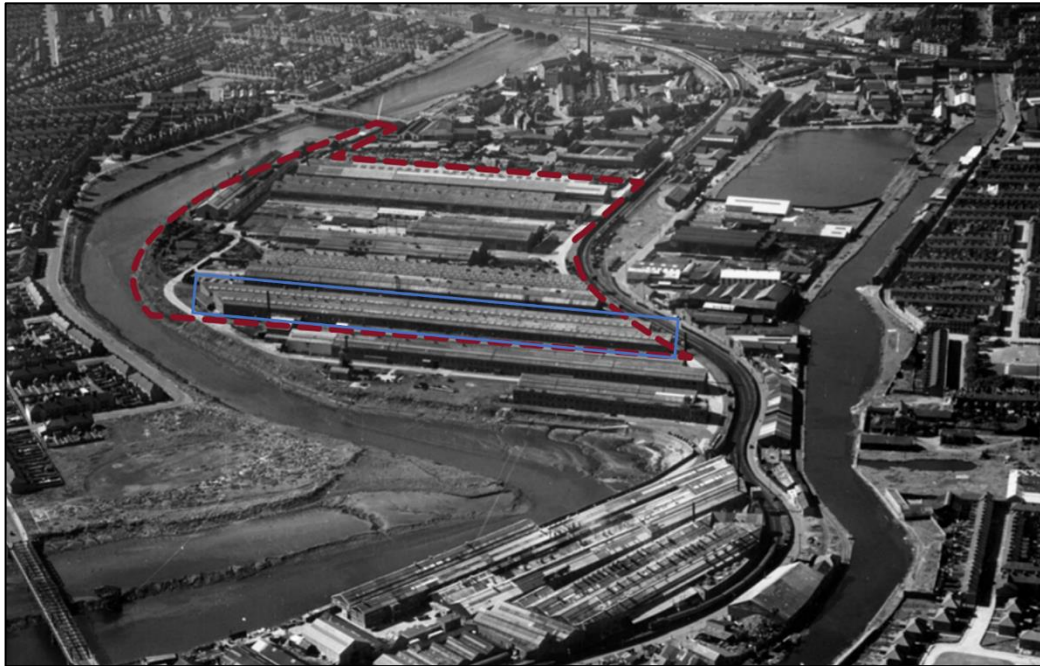


Figure 2.3: 1948 post-war aerial photograph, including the subject building (marked in blue)

³ Western Mail - Friday 02 January 1948

⁴ Cardiff The Biography, Dic Mortimer, 2014

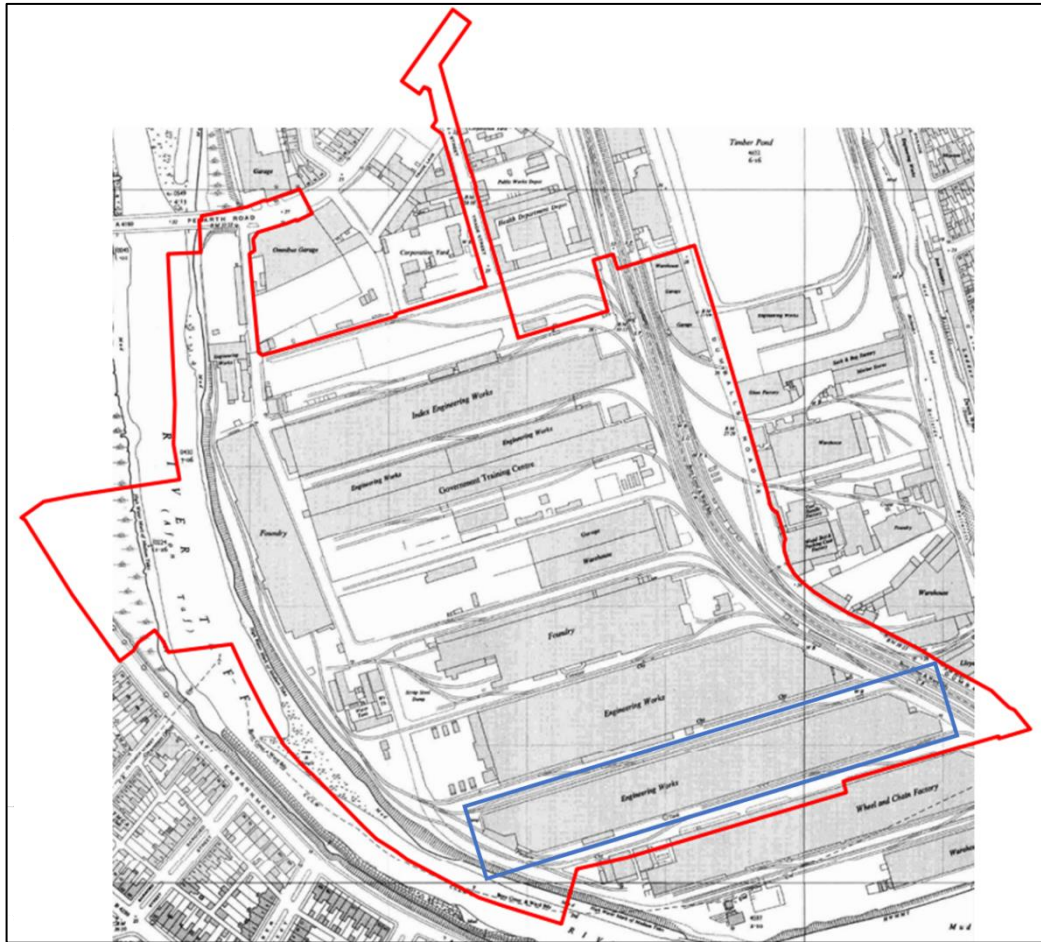


Figure 2.4: 1951-52 Ordnance Survey extract, 1:2,500 Scale Map (Site boundary red, DR017/Orion 5 Building blue)

Statement of Significance: DR017/Orion 5 Building

2.9 The subject building is not Locally Listed or understood to be a non-designated historic asset identified by the Council. The DBA assessment (Orion 2020) identified the structure as being of a low level of heritage significance, relating to its historic usage in the WW2 war effort:

“[The subject building is a] large linear two storey red-brick building which is the only surviving industrial unit associated with the Curran Engineering Works. The structure is in a disused and poor condition and the interior was largely not accessible. While some of the original features survive, particularly on the eastern and southern elevation, the building has undergone significant alteration including the loss of many of the original window along the south elevation, modification of the original roof with a raised sheet roof and the removal of much of the original industrial fittings (it was possible to view the central part of the building from the northern elevation which confirmed that this area was stripped out/derelict inside).”

2.10 The 1940s building follows a two-storey linear plan form, extending over approximately 340m at its widest point. It comprises two long bays with pitched roofs supported by trusses, which would originally have been partitioned and subdivided within, separating the specific functions of individual areas. Additional smaller single storey bays extend

from the centre of the north elevation and the west side of the south elevation. Notable later extensions and adaptations to the original building are evident in the raised corrugated iron roof and shed structures attached at the building's western end, as well as modern steel framed vehicle entrances inserted regularly along the north elevations.

- 2.11 Outer walls of the original structure are predominantly constructed of red brick with corrugated iron roofs supported by metal trusses. Walls have been painted over in blue and white over a select area at the western end of the building's south elevation. Later corrugated iron extensions are also evident.
- 2.12 The north elevation includes a formal tripartite entrance with white painted concrete architraves at its eastern end, otherwise continuing with regularly spaced metal framed multi-paned windows, of which many have been covered over or filled in with breeze blocks, with loss to glazing. Modern vehicle entrances with steel frames and shutters have been inserted at various points, interrupting the original architectural composition.
- 2.13 The west and east sides of the building are marked by angular structures, addressing the riverbank and Curran Road respectively, of an unusual profile in relation the rest of the building.
- 2.14 The south elevation continues with the same material palate as found elsewhere, with similar fenestration broken up by modern additions and smaller pedestrian entrances, set at different parts of the building with concrete lintels. A single ramp and raised vehicle entrance are positioned at the east end. Iron railings remain over select lengths of the outer roofline, though have been removed in most places.

Architectural and Historic Interest

- 2.15 The structure is of standard manufacture and of limited architectural or aesthetic value, being a comparatively plain, functional structure. Building materials used are prosaic and common to this period, with many parts the original fabric altered, damaged, or removed. The building has some illustrative/evidential value in connection with its function during the Second World War, though this has been eroded substantially due to the loss of original fabric and fixtures, particularly within the interior. Remaining sections of track, formerly used for transportation of products and materials and preserved within the road surface adjacent, show a very small level of illustrative value.
- 2.16 The building has historic interest as part of a key wartime phase in the development of the Curran Embankment, when from 1942 a new 100,000 square foot munitions complex was built. The building forms the only remaining part of this complex. Additionally, the building has some interest in connection with its post-Second World War uses, having been adapted by Curran Steels Ltd for the assembly of a variety of engineering/industrial products in the 1950s. Appreciation of the historic interest of the building has been significantly lessened, given its poor condition, reduced state of intactness, and modern context.

Contribution of Setting

- 2.17 Historically, the building formed part of a wider 1940s industrial and munitions complex extending over a much wider area at the Curran Embankment. The building is now the last remaining element of a series of contemporaneous former warehouses and is now surrounded by later development, including modern warehouse structures. The building

is contained to the far south of the Site, with the linear warehouse to the north similar in its proportions, with angularly composed elevations to the east addressing the Curran Road of a like shape, indicating it was built to the footprint and scale of an early 1940s warehouse. 20th century OS Maps also confirm this. There is nothing preserved within the building's setting, aside from a few isolated and fragmentary historic features, indicative of the former 1940s munitions infrastructure. No group value can be assigned to the building in relation to the industrial context or the wider townscape.

- 2.18 The River Taff extends across the west and southern sides of the subject building and is aligned by mature vegetation and trees. Residential development at Overstone Court is contained to the south and the CAVC Trade Campus building to the south east. New residential development is also under construction to the east on the opposite side of the Dumballs Road. The building's proximity to large-scale surrounding development and the well-defined boundary of the Site substantially reduces its visibility within the wider townscape. The full scale of the building can only be experienced from adjacent paths within the confines of the Curran Embankment, though with some partial views from outside on the publicly accessible Dumballs Road.

3. Photographic Record

- 3.1 The Photographic Record (Volume 2) is provided as a separate, stand-alone volume to accompany this Written Record (Volume 1).
- 3.2 As noted within **Section 1** of this report, the Photographic Record (Volume 2) includes:
- General views of the building in its wider setting;
 - The building's external appearance;
 - External details, structural or decorative, relevant to the building's design, development, and use.
- 3.3 The photographic record of the building was undertaken on the 9th August 2023, prior to any works commencing at the Site. The selected images have been chosen based on ensuring the best possible representation of the building and / or its features.
- 3.4 Internal access and photography were not possible given the building is in a poor structural condition/state of repair and contaminated with asbestos throughout; it was, therefore, not possible to secure safe access at this time.
- 3.5 The photos have been linked back an accompanying plan, with appropriate nomenclature, to identify the location of the photographer and direction of view. All photographs have been referenced and numbered accordingly.

Appendix 1: Written Scheme of Investigation

Written Scheme of Investigation
Land at Curran Embankment,
Cardiff

November 2022

Contents

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1. Introduction

1.1 This Written Scheme of Investigation (WSI) has been prepared by Turley Heritage on behalf of Vastint UK Services Ltd (hereafter referred to as the ‘applicant’), with regard to a building (DR017 in the GGAT report and Orion 5 in the Orion report), formerly used by the Curran Works for shell and cartridge casing production, on Land at the Curran Embankment, Butetown, Cardiff (**Figure 1.1**).

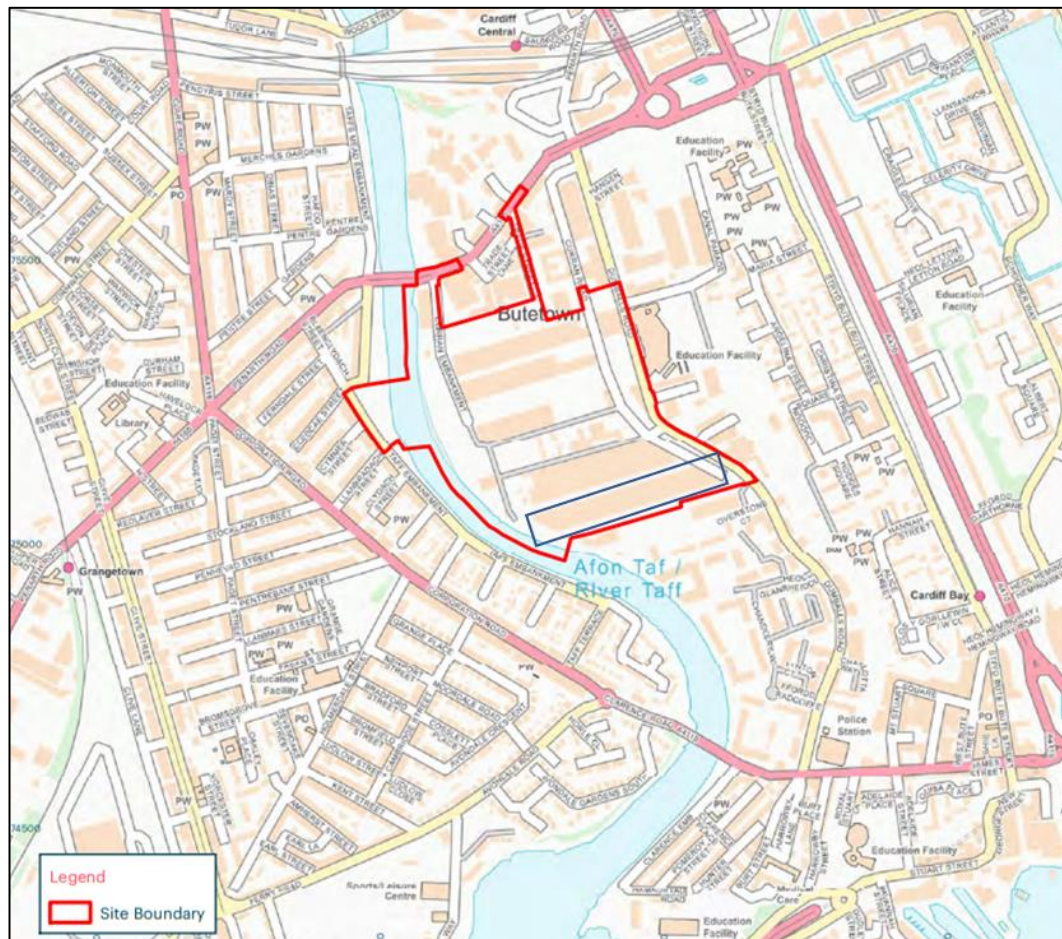


Figure 1.1: Site plan (DR017/Orion 5 building marked in blue)

1.2 A planning application was submitted on 26th March 2021 (ref. no. 21/00783/MJR) for:

“Demolition of existing buildings/structures and the comprehensive mixed use redevelopment to provide up to 2,500 new homes (Class C3), business space (Class B1) and a mix of complementary leisure, food and drink, hospitality, retail and health and wellbeing uses (Class A1, A2, A3, C1, D1 and D2); creation of new open space (including a new riverside park and water taxi stop); new pedestrian, cycle and vehicular access points; pedestrian footbridge; vehicular and cycle parking facilities; landscaping; public realm and other associated ancillary and highways works. All matters reserved other than vehicular access.”

- 1.3 This WSI has been prepared to establish an agreed basis to undertake a proportionate programme of Building Recording. This process has been agreed in correspondence with GGAT (dated 25th May 2021), who in response to this application, stated that the proposal had an archaeological restraint such that:

“[a] building has been identified (DR017 in the GGAT report and Orion 5 in the Orion report) which was part of the Curran Works, producing shell and cartridge casings. Whilst the structure has clearly undergone subsequent alterations, original features survive.

As such it is our recommendation that the structure be recorded prior to the commencement of the development.”

- 1.4 This WSI outlines a Level 3 survey¹ to be undertaken before the commencement of any works, setting out the structure and content of the Building Record that will be required in connection with the demolition proposals (ref: 21/00783/MJR).

¹ Historic England 2016, Understanding Historic Buildings: A guide to Good Recording Practice

2. Background of the Written Scheme of Investigation

- 2.1 The historic development of the Site is set out in the Heritage Statement, published by Orion Heritage Ltd (March 2021), that accompanied the application for planning permission (21/00783/MJR). The findings are provided as context for the WSI.

Historic Development of the Site and local context

- 2.2 Cardiff (Caerdydd in Welsh) began life as a Roman fort ('caer' meaning fort or castle). According to Lewis (1849), Norman invaders under Robert FitzHamon, a relative of the Conqueror, seized control of the Welsh lowlands of Glamorgan, and he constructed a stone castle at Cardiff in the 1090s. The town of Cardiff grew up around it. Robert, Earl of Gloucester, a son of King Henry I, became lord of Glamorgan through his marriage to Robert FitzHamon's daughter Mabel. In 1147, the earl established the Benedictine priory of St Mary that stood at the south end of St Mary Street. King Henry II stopped at Cardiff on his way to invade Ireland in 1172, while his son, Prince John, acquired the lordship of Glamorgan when he married Isabel, Countess of Gloucester in 1176. By the end of the 13th century, Cardiff had grown to become the largest town in Wales. Weekly markets were held while its port was authorised to trade in wool and leather. Owain Glyndwr besieged and temporarily captured the walled market town in 1404 during the Welsh Revolt/War of Independence. He made himself master of the castle and carried off treasure that had been deposited there for safekeeping.
- 2.3 The shire of Glamorgan was created in the 1530s following the Act of Union between England and Wales. St Mary's Priory was dissolved by Henry VIII, and the town was divided between the parishes of St John in the north and St Mary comprising the southern part of the town and Cardiff Bay. The flooding of the River Taff washed away St Mary's Church in 1607, and it was not rebuilt until the 1840s. King James I granted the town a royal charter in 1608 establishing Cardiff as a borough. The fortress was garrisoned for the king during the Civil War, and Charles I spent several nights at the castle in August 1645. By the 18th century, Cardiff was still a relative backwater with a population in 1800 of around 2,000 inhabitants. The opening of the Glamorganshire Canal in 1798 and the Taff Valley Railway in 1841 improved communication with the rest of Britain, though it was Cardiff's access to the sea that would turn its fortunes and transform it into a world-renowned city.



Figure 2.1: 1760 A New and Accurate Map of South Wales by Emmanuel Bowen

2.4 The development of the city in the 19th century fell largely within the purview of one aristocratic family, the Crichton-Stuarts, Marquesses of Bute. The land to the south of Cardiff between the town centre and the sea formed part of Lord Bute's vast estates in Wales. The area traditionally had been agricultural land, but according to Davies (1981) the idea for a new dock had been mooted in 1822. The neglected Bute estate was then embroiled in lawsuits, but by the 1830s, Lord Bute was able to redirect his attention to the dock development project that was planned to rival Liverpool. The dock would provide a convenient shipping outlet for coal from the Bute mineral estates in the Welsh valleys. Work began on improving the Glamorganshire Canal and constructing feeder

channels to the port, which passed just to the east of the study area, then the main development began in earnest with the construction of the West Bute Dock during 1834-39. Despite initial financial difficulties, demand for space grew quickly and as many as 160 vessels could be spotted at the docks at a time. In 1856-58, the Bute estate expanded the docks with a new facility of forty-six acres providing nearly two miles of quays and bringing overall capacity of the docks at Cardiff to two million tons of goods per year.²

2.5 The second half of the 19th century saw not just the expansion of the docks, but also the construction of a huge industrial estate with factories, workshops, and warehouses. This development also required new areas of housing for the thousands of dock and factory workers. Though much of this housing went up in Grangetown on the west bank of the River Taff, there were also new terraces built along Bute Road, between the study area and the West Bute Dock, as shown on the 19th century OS plans of the city. This whole area became known as Butetown. Meanwhile the old part of Cardiff was reworked into a grand Victorian metropolis.

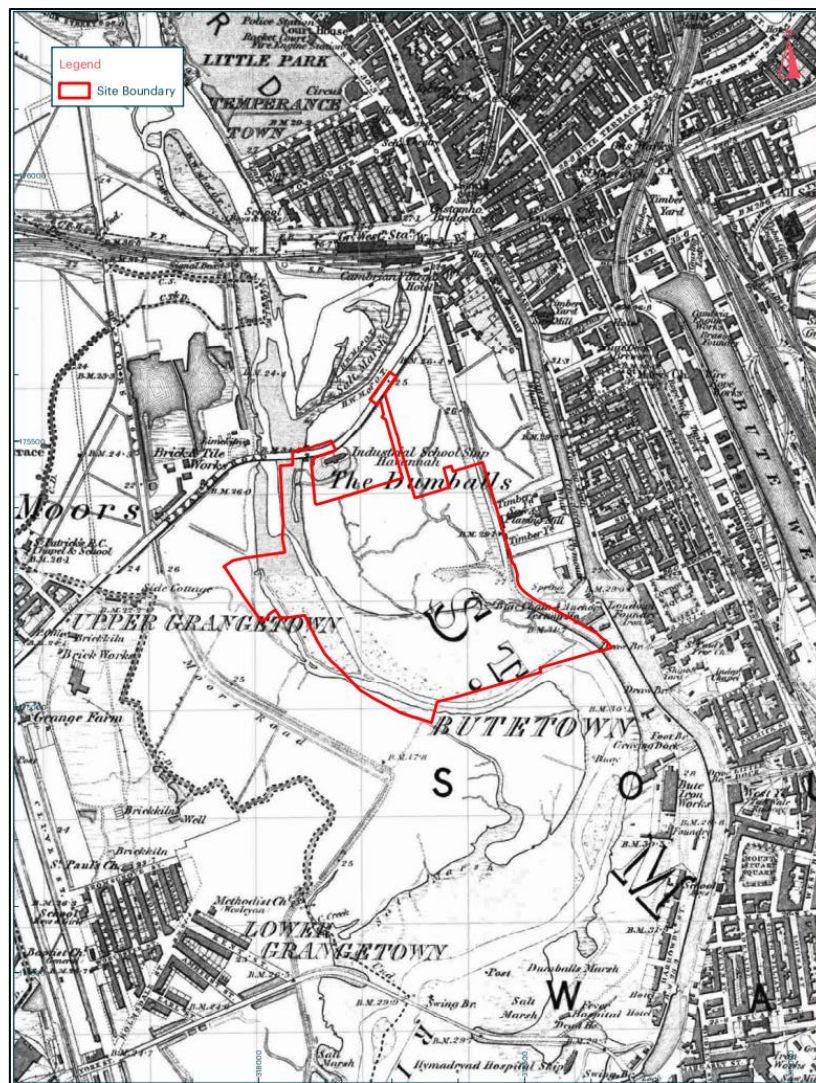


Figure 2.2: 1879-81 Ordnance Survey, 1:10,560 Scale Map

² Davies, 1981

- 2.6 As demand for coal continued to grow during the latter part of the 19th century, the Bute docks became the largest coal port in the world. Every bit of space between the River Taff and the sea was claimed for development, but the Bute Company was running out of land, so they began reclaiming land from the foreshore. The Queen Alexandra Dock, built on reclaimed land at the southern end of the peninsula, opened in 1907 providing accommodation for larger, modern vessels. Exports reached their peak in 1913 when more than thirteen million tons of coal left Cardiff docks.³ Despite rapid growth and development during the 19th century, generating huge wealth for the Crichton-Stuarts, during the early 20th century, the city's fortunes began to decline as other ports competed to carry Welsh coal to markets around the globe. Cardiff nonetheless remained an important industrial centre right through the Second World War and post-war period.
- 2.7 Cardiff was granted city status in 1905, and in the 1950s it beat out several rivals to be designated the capital of Wales. This took on greater significance at the end of the 20th century, when the process of devolution divided the governance of Wales between the UK parliament and the Welsh National Assembly, housed on a site in Cardiff Bay less than a mile south of the study area.

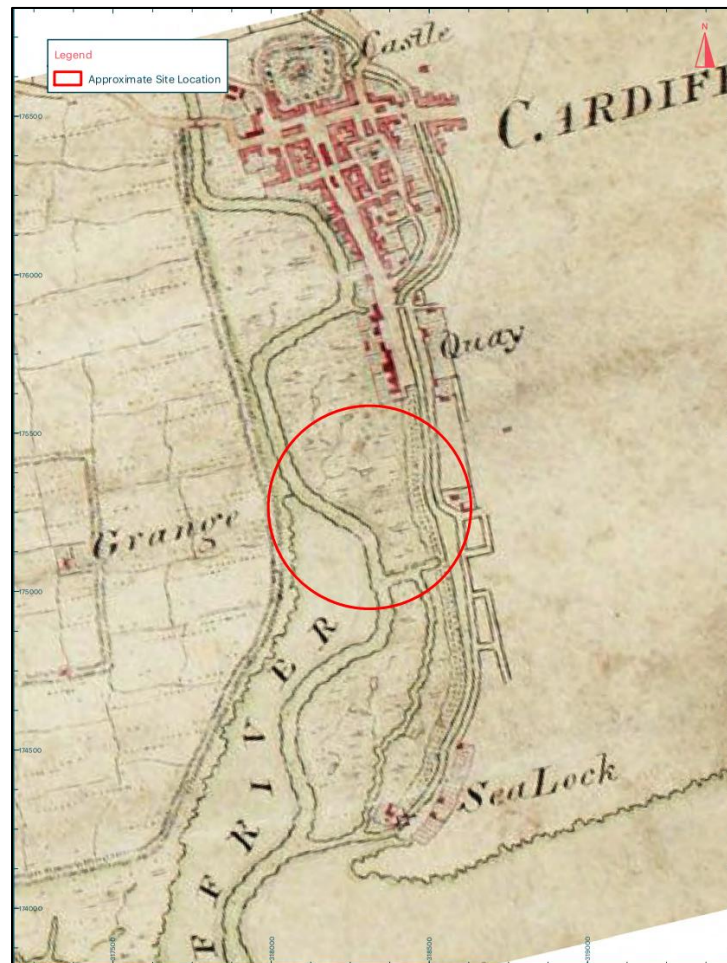


Figure 2.3: 1811 Ordnance Survey Drawing

³ Davies, 1981

2.8 In 1844 according to the tithe map and apportionment, the study area was still undeveloped salt marsh that Lord Bute had leased to Thomas Rees for pasturing. Rees also held several more parcels of meadow and pasture in Bute estate land on the opposite bank of the Taff. The first wooden bridge across the river was constructed in 1850 just to the north of the study area, and this encouraged the growth of the suburbs at Grangetown and Penarth. A decade later, Cardiff's ragged school, which educated destitute children in the town, petitioned the Admiralty for an old ship so the school committee could open an industrial school where juvenile vagrants would receive an education.⁴ HMS Havannah was donated on condition that the committee paid for the ship to be towed from Plymouth to Cardiff. She was a fifth rate frigate fitted with thirty-six guns that had been launched at Liverpool in 1811 and saw service during the Napoleonic Wars.⁵ Havannah underwent conversion in East Bute Dock and was later moved to a position on the moor at the south side of the Penarth bridge. Here the ship was floated by the spring tides and the movement repeatedly broke the ship's connection to corporation gas and water supplies, so eventually an embankment was erected around it, as shown on the 1880 OS plan.

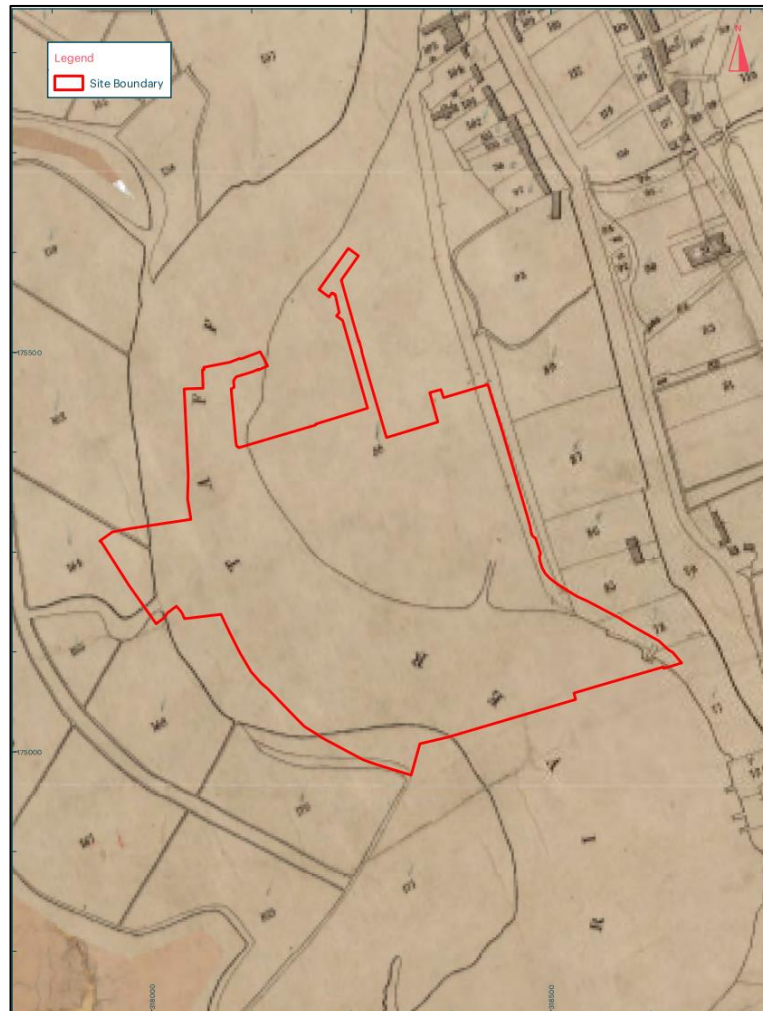


Figure 2.4: 1844 Tithe Survey of St Mary's Parish in Cardiff

⁴ Higginbotham, 2020

⁵ Colledge, 2003

2.9 Havannah was officially certificated as an industrial school ship, the first of its kind, in December 1861. There was accommodation for ninety boys aged between ten and fourteen who were taught nautical skills should they desire a career in the navy as well as other skills to prepare them for gainful employment with local tradesmen (Higginbotham, 2020). By 1900 more than 600 boys had been educated on the hulk, and as noted in the local press the annual report of the industrial school for 1896 estimated that at least eighty-five percent of these had “turned out well” and become lawabiding citizens.⁶ Despite this favourable assessment, the school was closed down in 1904, and the ship was broken up the following year.⁷

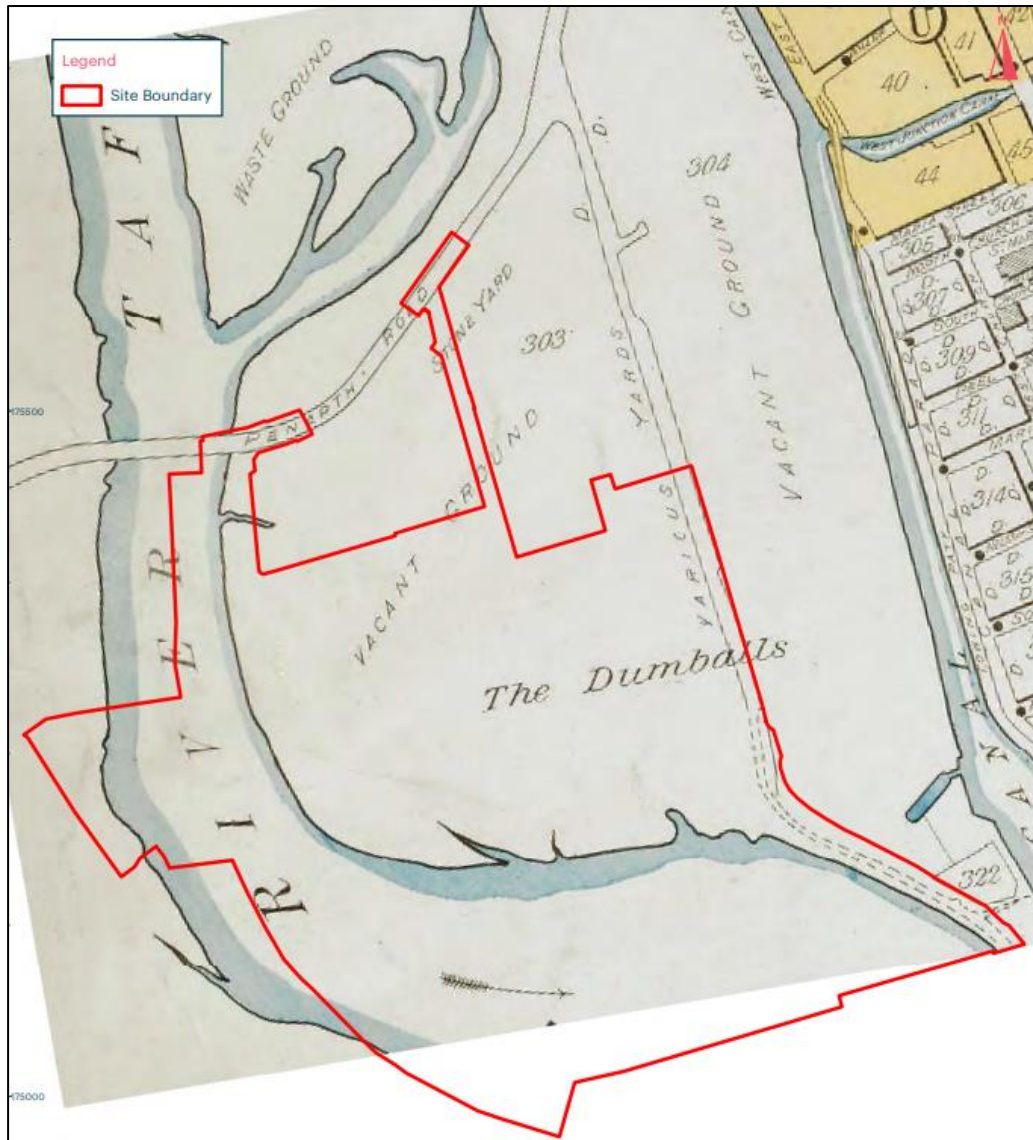


Figure 2.5: 1888 Goad Insurance Plan of Cardiff

⁶ 'Havannah Industrial School, Cardiff Annual Report', South Wales Daily News, 16 May 1896

⁷ Colledge, 2003

2.10 In 1878, an application was made to Parliament by the proprietors of the Glamorganshire Canal Navigation for a bill to authorise the company to construct a dock, basins, graving dock, quays, jetties, wharves and warehouses on the east side of the Glamorganshire Canal and on the west side between the canal and the River Taff.⁸ The bill would also authorise the company to construct a timber float or pond on the east side of the Dumballs Road to the north of the timber yard shown on the 1880 OS plan. This pond, however, was not sufficient and a second much larger pond was excavated on the west side of Dumballs Road at some point between 1880 and 1900. Here importers could store timber in water to prevent it seasoning before it could be processed at the neighbouring sawmill or sold on. As well as the two ponds belonging to the canal company, there was another timber pond adjacent to the Bute East Dock (shown on the 1900 OS plan) though this was only about half the size of the pond beside the river. The larger pond was filled in during the late 1930s to make way for a series of factories and engineering works each furnished with private sidings that connected them to the Cardiff Riverside Branch of the Great Western Railway.

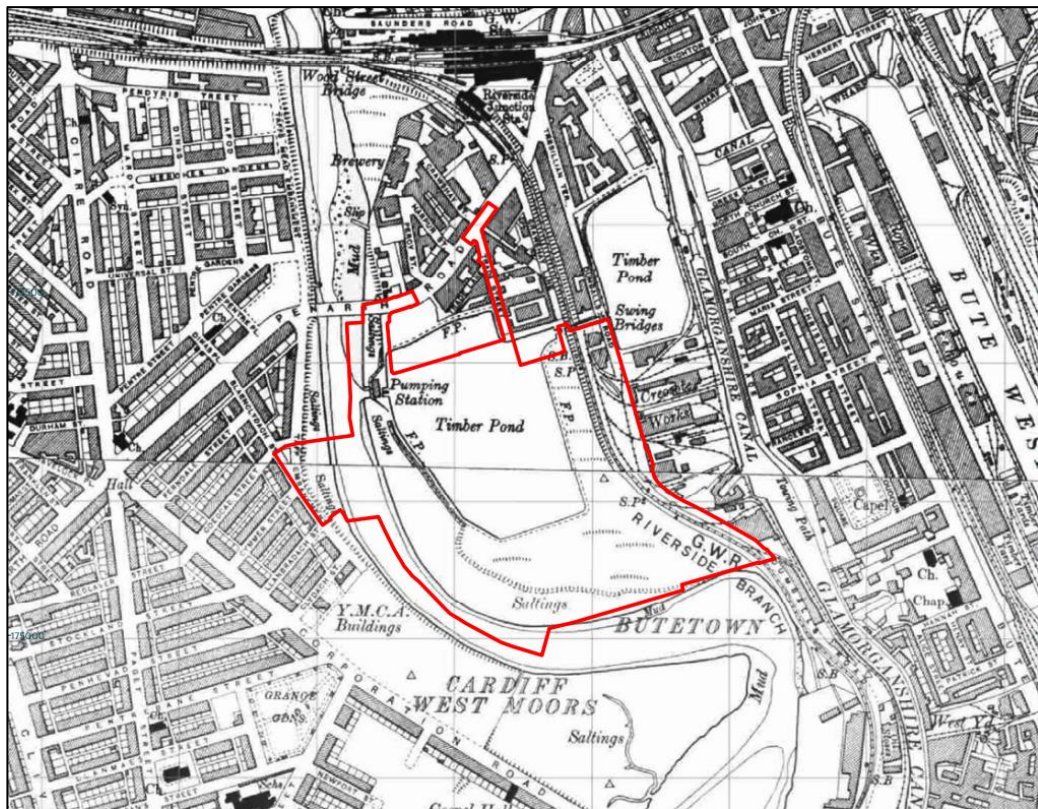


Figure 2.6: 1915-16 Ordnance Survey, 1:10,560 Scale Map

2.11 Probably the best-known occupant of the site was Curran Steels. Edward Curran had founded an engineering firm at Hurman Road in Butetown in the early 1900s.⁹

⁸ 'In Parliament – Session 1878', The London Gazette, 20 November 1878

⁹ 'The girls were as strong as men at armaments firm Currans', Wales Online, 28 February 2009



Figure 2.7: 1937 Aerial photograph

2.12 Curran produced munitions in the 1930s, helping to prepare Britain for the Second World War. The new works at Curran Road was opened in 1942 and included a 100,000 square foot steel foundry with laboratory, dressing shops and pattern shops, and a promotional article from 1959 stated that Curran Steels Ltd. was then manufacturing earthmoving equipment, industrial presses, castings and general engineering products at the works on Curran Road.¹⁰ The building (DR017/Orion 5) that is the object of this WSI was built as part of these new works that opened in 1942. The 1951-1952 OS Map shows that it was used as engineering works in combination with many other buildings within the Curran complex.

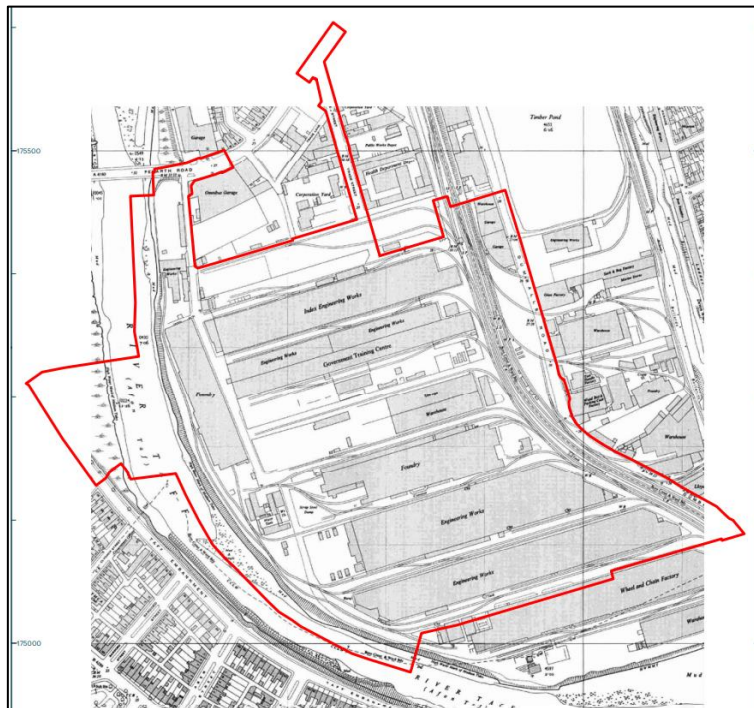


Figure 2.8: 1951-52 Ordnance Survey, 1:2,500 Scale Map

¹⁰ ('Company in Wales Offers Facilities', Foreign Commerce Weekly, 18 May 1959)

Policy and Guidance

2.13 In preparing this WSI, regard has been had to the guidance set out within Planning Policy Wales (Edition 11, February 2021), and Technical Advice Note (TAN) 24: The Historic Environment (2017), as the relevant policy and guidance basis for undertaking a historic building survey and photographic record in Wales. These policies specifically relate to the recording of listed buildings but can be used to give an indication of the correct approach for undertaking survey or photographic recording in other circumstances.

2.14 Paragraph 6.1.27 of 'Planning Policy Wales (Edition 11)' states that:

"If the planning authority is minded to approve an application and where archaeological remains are affected by proposals that alter or destroy them, the planning authority must be satisfied that the developer has secured appropriate and satisfactory provision for their recording and investigation, followed by the analysis and publication of the results and the deposition of the resulting archive in an approved repository. On occasions, unforeseen archaeological remains may still be discovered during the course of a development. A written scheme of investigation should consider how to react to such circumstances, or it can be covered through an appropriate condition for a watching brief. Where remains discovered are deemed to be of national importance, the Welsh Ministers have the power to schedule the site and in such circumstances scheduled monument consent must be required before works can continue."

2.15 Paragraph 5.27 'Building Recording' of 'Technical Advice Note (TAN) 24: The Historic Environment (2017)' states:

"Works undertaken as the result of a listed building consent will often present the opportunity to make a record of features of the building that are to be removed or altered, or which may be revealed during the course of works. Works associated with a listed building consent may also reveal archaeological remains of earlier phases of the building or items that are associated with the building. When considering a listed building consent application, the local planning authority must decide whether a programme of building recording and/or archaeological investigation is required to record the features that might be revealed or destroyed. They may seek the advice of their archaeological advisors and impose a suitable condition relating to recording (see 4.13). Applicants should contact the local planning authority when unexpected discoveries are made during the course of works. The Royal Commission on the Ancient and Historical Monuments of Wales must be given an opportunity to record a listed building proposed for demolition."

2.16 Further specific guidance has also been referred to, however, in lieu of any specific Welsh guidance on the production of recording reports (i.e., by Cadw), the content and structure of the proposed recording report is drawn from Historic England's best practice guidance; 'Understanding Historic Buildings: A Guide to Good Recording Practice (Historic England, 2016)'.

2.17 The RCAHMW's Guidelines for Digital Archaeological Archives (March 2015)

2.18 The Building Record will be undertaken by Turley Heritage.

3. Content and Structure of Photographic Record

Introduction

- 3.1 The purpose of a building record is to provide an understanding of the building and to document parts of the building at a particular point in time, particularly prior to demolition. Most records will incorporate a written description and analysis, drawing on an investigation of the building's fabric, research of documentary sources and a visual record including photography and/or drawings.
- 3.2 In this instance, evidence has demonstrated that the 1940s building is of sufficient interest to warrant a Level 3 Survey.¹¹ This approach has been confirmed in conjunction with pre-application dialogue with GGAT and the Council.
- 3.3 As established in the guidance:

“Level 3 is an analytical record, and will comprise an introductory description followed by a systematic account of the building’s origins, development and use. The record will include an account of the evidence on which the analysis has been based, allowing the validity of the record to be re-examined in detail. It will also include all drawn and photographic records that may be required to illustrate the building’s appearance and structure and to support an historical analysis. The information contained in the record will for the most part have been obtained through an examination of the building itself. The documentary sources used are likely to be those which are most readily accessible, such as historic Ordnance Survey maps, trade directories and other published sources. The record may contain some discussion of the building’s broader stylistic or historical context and importance. It may form part of a wider survey of a number of buildings which will aim at an overall synthesis, such as a thematic or regional publication, when the use of additional source material may be necessary as well as a broader historical and architectural discussion of the buildings as a group. A Level 3 record may also be appropriate when the fabric of a building is under threat, but time or resources are insufficient to allow for detailed documentary research, or where the scope for such research is limited.”¹²

- 3.4 Section 4 of the Historic England Guidance (Creating a Record) gives detailed guidance on the types of information required for different types of record. The minimum requirements for a Level 3 Record are:

Drawings

- 3.5 Drawings for a Level 3 Recording will include Measured plans (to scale or fully dimensioned) as existing. These may extend to all floors, or they may be restricted to one or a selection. The latter option may be appropriate, for example, in a town-centre building where an upper floor has been little altered. Buildings with a repetitive structure may also be planned on one floor, but a note or a sketch plan should be made to indicate the arrangement of other floors. Plans should show the form and location of any structural features of historic significance, such as blocked doorways, windows and

¹¹ Historic England: Understanding Historic Buildings: A guide to Good Recording Practice 2016

¹² Historic England: Understanding Historic Buildings: A guide to Good Recording Practice 2016

fireplaces, masonry joints, ceiling beams and other changes in floor and ceiling levels, and any evidence for fixtures of significance.

Photographs

3.6 Site photography for a Level 3 Building Recording includes the following:

- A general view or views of the building (in its wider setting or landscape if 2 (below) is also to be adopted).
- The building's external appearance. Typically a series of oblique views will show all external elevations of the building, and give an overall impression of its size and shape. Where individual elevations include complex historical information it may also be appropriate to take views at right-angles to the plane of the elevation.
- Further views may be desirable to reflect the original design intentions of the builder or architect, where these are known from documentary sources or can be inferred from the building or its setting.
- The overall appearance of the principal rooms and circulation areas.
- Any external or internal detail, structural or decorative, which is relevant to the building's design, development and use, with scale where appropriate.
- Any machinery or other plant, or evidence for its former existence.
- Any dates or other inscriptions; any signage, makers' plates or graffiti which contribute to an understanding of the building. A transcription should be made wherever characters are difficult to interpret.
- Any building contents which have a significant bearing on the building's history (for example, a cheese press, a malt shovel).
- Copies of maps, drawings, views and photographs, present in the building and illustrating its development or that of its site. The owner's written consent may be required where copies are to be deposited in an archive.

Report

3.7 In accordance with Historic England's best practice guidance¹³, the written account will comprise:

- The precise location of the building as an address and in the form of a National Grid reference.
- A note of any statutory designation (that is, listing, scheduling, Register of Historic Parks and Gardens, conservation area). Information on statutory designations can be found on the Cadw website. Non-statutory designations (local lists) may be added.

¹³ Historic England: Understanding Historic Buildings: A guide to Good Recording Practice 2016

- The date when the record was made, the name(s) of the recorder(s) and the location of any archive material.
- A longer summary statement. This account should summarise the building's form, function, date and sequence of development. The names of architects, builders, patrons and owners should be given if known. Its purpose is to describe the building when no fuller record is necessary. Alternatively it may serve as an introduction to the more detailed body of a record that may follow, for users who may need a summary of the report's findings.
- An introduction briefly setting out the circumstances in which the record was made, its objectives, methods, scope and limitations, and any constraints. Where appropriate the brief for the work or the project design should be stated or appended.
- Acknowledgements to all those who have made a significant contribution to the making of the record, or who have given permission for copyright items to be reproduced.
- A discussion of the published sources relating to the building and its setting, an account of its history as given in published sources, an analysis of historic map evidence (map regression) and a critical evaluation of previous records of the building, where they exist.
- An account of the building's overall form (structure, materials, layout) and of its successive phases of development, together with the evidence supporting this analysis.
- An account of the building's past and present use, and of the uses of its parts, with the evidence for these interpretations. An analysis of a circulation pattern or of a decorative or liturgical scheme. An account of any fixtures, fittings, plant or machinery associated with the building, and their purpose. In an industrial building, a sequential account of the way in which materials or processes were handled.
- Any evidence for the former existence of demolished structures or removed plant associated with the building.
- Full bibliographic and other references, or a list of the sources consulted (in long reports it is preferable to include both). Websites which may prove to be ephemeral should be avoided as references wherever possible; where their use is unavoidable the full web address and the date on which the site was consulted should be noted.

3.8 Other elements identified by Historic England in this Section may also be relevant, as necessary, but are not considered to be essential for a Level 3 Building Recording Report.

Publication and Archive Submission

3.9 This overall approach is consistent with the requirements associated with planning permission (ref: 21/00783/MJR) as recommend by GGAT.

- 3.10 In accordance with best practice guidance, once the building record has been approved, one PDF version on a memory stick/flash drive will be submitted to the National Monuments Record held by the Royal Commission on Ancient and Historical Monuments of Wales (RCAHMW). Confirmation of receipt by the RCAHMW will be provided to the Council in order to discharge the condition.

Known Limitations

- 3.11 The survey will be limited to a visual inspection of the readily accessible areas of the Site and its local context.
- 3.12 Internal access and photography are not possible given the high safety risk imposed by the subject building's poor structural condition, state of repair and asbestos contamination throughout.

Turley Office
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12 Roger Street
London
WC1N 2JU

T 020 7851 4010

**Appendix 2: Planning Application 26/03/2021
(ref. no. 21/00783/MJR)**

Print Version

[Close Window](#)[Print](#)

Summary

Reference	21/00783/MJR
Alternative Reference	PP-09414134
Application Received	Fri 26 Mar 2021
Application Validated	Tue 06 Apr 2021
Address	Land At Curran Embankment Butetown Cardiff
Proposal	DEMOLITION OF EXISTING BUILDINGS/STRUCTURES AND THE COMPREHENSIVE MIXED-USE REDEVELOPMENT TO PROVIDE UP TO 2,500 NEW HOMES (CLASS C3), BUSINESS SPACE (CLASS B1) AND A MIX OF COMPLEMENTARY LEISURE, FOOD AND DRINK, HOSPITALITY, RETAIL AND HEALTH AND WELLBEING USES (CLASS A1, A2, A3, C1, D1 AND D2); CREATION OF NEW OPEN SPACE (INCLUDING A NEW RIVERSIDE PARK AND WATER TAXI STOP); NEW PEDESTRIAN, CYCLE AND VEHICULAR ACCESS POINTS; PEDESTRIAN FOOTBRIDGE; VEHICULAR AND CYCLE PARKING FACILITIES; LANDSCAPING; PUBLIC REALM AND OTHER ASSOCIATED ANCILLARY AND HIGHWAYS WORKS. ALL MATTERS RESERVED OTHER THAN VEHICULAR ACCESS
Status	Registered
Appeal Decision	

Further Information

Application Type	Outline Planning Permission
Expected Decision Level	
Case Officer	Amanda Sutcliffe
Ward	BUTETOWN
Applicant Name	Vastint UK Services Limited

Agent Name	Mr Jones
Agent Company Name	Turley
Agent Address	18 Windsor Place Cardiff CF10 3BY
Environmental Assessment Requested	Yes

Contacts

Agent

Mr Jones

Email	huw.jones@turley.co.uk
Phone	02920344445

Ward Councillors

Councillor Saeed Ebrahim

Address	Members Services Room 286, County Hall Atlantic Wharf Cardiff CF10 4UW
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Councillor Helen Gunter

Address	Members Services Room 286, County Hall Atlantic Wharf Cardiff CF10 4UW
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Councillor Margaret Lewis

Address	Members Services Room 286, County Hall Atlantic Wharf Cardiff CF10 4UW
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Important Dates

Application Received Date	Fri 26 Mar 2021
Application Validated Date	Tue 06 Apr 2021
Expiry Date	Sun 06 Feb 2022
Actual Committee Date	
Latest Neighbour Consultation Date	Wed 05 Jan 2022

Neighbour Consultation Expiry Date	
Standard Consultation Date	Fri 19 Aug 2022
Standard Consultation Expiry Date	
Last Advertised In Press Date	Thu 22 Apr 2021
Latest Advertisement Expiry Date	
Last Site Notice Posted Date	Fri 14 May 2021
Latest Site Notice Expiry Date	
Agreed Expiry Date	Thu 29 Sep 2022
Temporary Permission Expiry Date	

Related Information

There are 194 documents associated with this application.

There are 0 cases associated with this application.

There is 1 property associated with this application.

Appendix 3: Heritage Statement (Orion, March 2021)

Curran Embankment, Cardiff
Heritage Statement
March 2021

Curran Embankment, Cardiff
Heritage Statement
March 2021

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Report

Heritage Statement

Site

Curran Embankment, Cardiff

Client

Stantec UK Ltd

Planning Authority

Cardiff Council

Grid reference

318365 175418

Prepared By

Emma Baxter BA (Hons) MA (Cantab) MSc IHBC

Reviewed By

Dr Rob Smith MCIfA

Report Status

FINAL

Date

23/03/2021

Orion Ref

PN2620/HS/4

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Executive Summary

This heritage statement considers Curran Embankment, Cardiff (the study site) and the potential impact of the proposed development on the significance of non-designated heritage assets located within the subject site and those designated and non-designated heritage assets located in its vicinity.

In summary, the proposals are considered to be compliant with regard to National and Local Government policy.

All designated heritage assets have been demonstrated within this assessment as being preserved from harm to their significance.

TAN 24 states at paragraph 1.29: The local planning authority will need to make its own assessment of the impact within the setting of a historic asset, having considered the responses received from consultees as part of this process. A judgement has to be made by the consenting authority, on a case-by-case basis, over whether a proposed development may be damaging to the setting of the historic asset, or may enhance or have a neutral impact on the setting by the removal of existing inappropriate development or land use.

1.0 Introduction

- 1.1 This heritage statement considers Curran Embankment, Cardiff (Figure 1). The site (hereinafter referred to as the “Study Site”) is located at National Grid Reference 318365 175418.
- 1.2 In accordance with the Paragraph 189 of the *National Planning Policy Framework (NPPF 2018)* and the requirement for applicants to describe the significance of heritage assets including contribution to setting, the report draws together available information on designated and non-designated heritage assets. The assessment includes the results of a site survey, an examination of published and unpublished records, charts historic land-use through a map regression exercise and considers relevant local and national policy and guidance.
- 1.3 The Historic Environment Record has been consulted and the relevant designated and non-designated heritage assets located in the immediate vicinity are identified in Figure 2.
- 1.4 A site visit was undertaken on 7th October 2020 when the conditions were fair and visibility was good.
- 1.5 The report enables relevant parties to assess the significance of designated and non-designated heritage assets within and in the vicinity of the site, thus enabling potential impacts on these assets to be identified.

Location and Description

- 1.6 The study site occupies 13.24 hectares at an elevation of 8.8m above ordnance datum (AOD) in the Butetown area, to the south of Cardiff city centre. The site is bounded by a steep vegetated bank of the River Taff to the west, Dumballs Road and commercial/industrial units to the east, residential development to the south and commercial units to the north. Much of the site is currently active, with a mix of commercial, industrial and engineering practices across the site. Additionally, there are a number of derelict and empty warehouses on the site, which are not currently in use. The 1950s Curran Steelworks has the potential to be considered as a non-designated heritage asset by the LPA, although it is not currently identified as such, and has previously been considered within the DBA (Orion, June 2020). The study site and subject building are shown at (plates 1-9).



Plate 1: East elevation of mid-20th century industrial unit associated with the Curran Engineering Works (looking south)



Plate 2: East elevation of mid-20th century industrial unit associated with the Curran Engineering Works (looking north)



Plate 3: South elevation of mid-20th century industrial unit associated with the Curran Engineering Works (looking north west)



Plate 4: Northern elevation of mid-20th century industrial unit associated with the Curran Engineering Works (looking south east)



Plate 5: South elevation of mid-20th century industrial unit associated with the Curran Engineering Works (looking east)



Plate 6: Surface evidence associated with the GWR (Riverside Branch) along Curran Road in the north-east of the study site (looking south)



Plate 7: Hardstanding associated with the former foundry site adjacent to the embankment (looking south)



Plate 8: Hardstanding associated with the former foundry site adjacent to the embankment (looking north)



Plate 9: Railway lines at the south west of the study site, close to the location of the former foundry (looking west towards the River Taff)

2.0 Planning Background and Development Plan Framework

- 2.1 The Ancient Monuments and Archaeological Areas Act 1979, Planning (Listed Buildings and Conservation Areas) Act 1990 and Historic Environment (Wales) Act 2016 provide the legislative framework for the protection and sustainable management of the historic environment in Wales. PPW provides the national planning policy framework for the consideration of the historic environment and this is supplemented by guidance contained in Technical Advice Note 24: The Historic Environment and Cadw associated best practice guidance on the historic environment.

Planning (Listed Building and Conservation Areas) Act 1990

- 2.2 The *Planning (Listed Building and Conservation Areas) Act 1990* sets out broad policies and obligations relevant to the protection of Listed Buildings and Conservation Areas and their settings.

- 2.3 Section 66(1) states:

In considering whether to grant planning permission for development which affects a listed building or its setting, the local planning authority or, as the case may be, the Secretary of State shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses.

- 2.4 Section 69 of the Act requires local authorities to define as conservation areas any *areas of special architectural or historic interest the character or appearance of which it is desirable to preserve or enhance* and Section 72 gives local authorities a general duty to pay special attention to *the desirability of preserving or enhancing the character or appearance of that area* in exercising their planning functions. These duties are taken to apply only within a Conservation Area. The Act does not make specific provision with regard to the setting of a Conservation Area, that is provided by the policy framework outlined below.

Historic Environment (Wales) Act

- 2.5 The Historic Environment (Wales) Act was given Royal Assent in March 2016. This Act provides the legislative framework for managing the historic environment in Wales. Accompanying the Act is new policy and guidance in the form of a Technical Advice Note (TAN) specific to the Historic Environment (see below), and changes to Planning Policy Wales (PPW) Chapter 6 – Conserving the Historic Environment.

Planning Policy Wales

- 2.6 The Welsh Government has published Planning Policy Wales (PPW), currently updated to Version 11 from February 2021. This sets out the land use planning policies of the Welsh Government. It is supplemented by a series of Technical Advice Notes (TANs). Procedural advice is given in circulars and policy clarification letters.

- 2.7 Section 6.1 of PPW, entitled ‘The Historic Environment’, provides policy for planning authorities, property owners, developers and others on the conservation and investigation of historic assets. Section 6.1 sets out the Welsh Governments specific objectives for the historic environment as seeking to:
- protect the Outstanding Universal Value of the World Heritage Sites;
 - conserve archaeological remains, both for their own sake and for their role in education, leisure and the economy;
 - safeguard the character of historic buildings and manage change so that their special architectural and historic interest is preserved;
 - preserve or enhance the character or appearance of conservation areas, whilst the same time helping them remain vibrant and prosperous;
 - preserve the special interest of sites on the register of historic parks and gardens; and
 - protect areas on the register of historic landscapes in Wales.

- 2.8 Section 6.1 of PPW defines the historic environment as:

The historic environment comprises all the surviving physical elements of previous human activity and illustrates how past generations have shaped the world around us.

Technical Advice Note (TAN) 24: The Historic Environment

- 2.9 The purpose of the TAN is to provide guidance on how the planning system considers the historic environment during development plan preparation and decision making on planning and Listed Building applications. This TAN provides specific guidance on how the following aspects of the historic environment should be considered:
- World Heritage Sites;
Scheduled Monuments;
Archaeological remains;
Listed Buildings;
Conservation Areas;
Historic Parks and Gardens;
Historic Landscapes; and
Historic assets of special local interest.
- 2.10 The following relevant aspects are covered in detail:
- 2.11 Planning Policy Wales identifies how local planning authorities must treat World Heritage Sites, scheduled monuments, unscheduled nationally important archaeological remains, listed buildings, conservation areas and registered historic parks and gardens in Wales in their consideration of planning applications and producing development plans. This includes the impact of proposed developments within the settings of these historic assets.
- 2.12 Local planning authorities are required to consult the Welsh Ministers, through Cadw, on certain development proposals which affect scheduled monuments, registered historic parks, gardens and landscapes and the Outstanding Universal Value of World Heritage Sites and their settings. It is for the local planning authority to consider the effects of proposed developments within the settings of listed buildings, conservation areas, unscheduled monuments of national importance and other undesignated historic assets.
- 2.13 The setting of an historic asset includes the surroundings in which it is understood, experienced, and appreciated embracing present and past relationships to the surrounding landscape. Its extent is not fixed and may change as the asset and its surroundings evolve. Elements of a setting may make a positive or negative contribution to the significance of an asset, may affect the ability to appreciate that significance or may be neutral. Setting is not a historic asset in its own right but has value derived from how different elements may contribute to the significance of a historic asset.
- 2.14 The local planning authority will need to make its own assessment of the impact within the setting of a historic asset, having considered the responses received from consultees as part of this process. A judgement has to be made by the consenting authority, on a case-by-case basis, over whether a proposed development may be damaging to the setting of the historic asset, or may enhance or have a neutral impact on the setting by the removal of existing inappropriate development or land use.

Conservation Principles for the Sustainable Management of the Historic Environment in Wales 2011

- 2.15 This document provides the basis upon which Cadw discharges certain statutory duties on behalf of the Welsh Ministers. Conservation Principles should be used by others (including owners, developers and other public bodies) to assess the potential impacts of a development proposal on the significance of any historic asset/assets and to assist in decision making where the historic environment is affected by the planning process.
- 2.16 There are six principles:
- 1) Historic assets will be managed to sustain their values;
 - 2) Understanding the significance of historic assets is vital;
 - 3) The historic environment is a shared resource;
 - 4) Everyone will be able to participate in sustaining the historic environment;
 - 5) Decisions about change must be reasonable, transparent and consistent; and
 - 6) Documenting and learning from decisions is essential.
- 2.17 In considering any planning application for development, the planning authority will be mindful of the framework set by government policy, by current Development Plan Policy and by other material considerations.

Local Planning Policy

- 2.18 In January 2016, the Council adopted the Cardiff Local Development Plan 2006-2026. The Local Development Plan 2011-2026 contains the following policies relevant to the historic environment:

KP17: BUILT HERITAGE

Cardiff's distinctive historic assets will be protected, managed and enhanced, in particular the character and setting of its Scheduled Ancient Monuments; Listed Buildings; Registered Historic Landscapes, Parks and Gardens; Conservation Areas; Locally Listed Buildings and other features of local interest that positively contribute to the distinctiveness of the city.

EN9: CONSERVATION OF THE HISTORIC ENVIRONMENT

- 2.19 Development relating to any of the historic assets listed below (or their settings) will only be permitted where it can be demonstrated that it preserves or enhances that asset's architectural quality, historic and cultural significance, character, integrity and/or setting. i. Scheduled Ancient Monuments; ii. Listed Buildings and their curtilage structures; iii. Conservation Areas; iv. Archaeologically Sensitive Areas; v. Registered Historic Landscapes, Parks and Gardens; or vi. Locally Listed Buildings of Merit and other historic features of interest that positively contribute to the distinctiveness of the city.

Definitions and Guidance

Definition of the Historic Environment

- 2.20 There is no single agreed definition of the historic environment or its components. For the purposes of this assessment, useful definitions are found in the Cadw Conservation Principles for the Sustainable Management of the Historic Environment in Wales (March 2011). In this document, the historic environment is defined as:

- 2.21 All aspects of the environment resulting from the interaction between people and places through time, including all surviving physical remains of past human activity, whether visible, buried, or submerged, and deliberately planted or managed.

Cadw Setting Guidance

- 2.22 Guidance on setting in Wales is provided by the Cadw Setting of Historic Assets in Wales (2017). This guidance defines setting as follows:

The setting of a historic asset includes the surroundings in which it is understood, experienced and appreciated, embracing present and past relationships to the surrounding landscape. Its extent is not fixed and may change as the asset and its surroundings evolve. Elements of a setting may make a positive, negative or neutral contribution to the significance of an asset.

- 2.23 The guidance also provides a four step process for the assessment of setting:
- Stage 1: Identify the historic assets that might be affected by a proposed change or development.
 - Stage 2: Define and analyse the settings to understand how they contribute to the significance of the historic assets and, in particular, the ways in which the assets are understood, appreciated and experienced.
 - Stage 3: Evaluate the potential impact of a proposed change or development on that significance.
 - Stage 4: If necessary, consider options to mitigate or improve the potential impact of a proposed change or development on that significance.
- 2.24 This staged process allows the setting of the historic asset to be understood and for development proposals to be responsive to their settings where potential impacts may occur.

Cardiff Council Supplementary Planning Guidance

Tall Buildings (2017)

- 2.25 The supplementary planning guidance requires the siting and location of tall buildings to have regard to the skyline and key views in relation to the townscape and heritage assets. This SPG is used in conjunction with the relevant Conservation Area Appraisals to inform the assessment of levels of impact to the historic environment which may potentially arise as a result of the proposed development.

3.0 Historical Context and Location of Heritage Assets

Introduction

- 3.1 The following section presents a historical development of the site and wider area through the results of a map regression exercise and review of relevant background documentation.
- 3.2 The location of designated and non-designated heritage assets within and surrounding the site are also discussed below; these are shown on Figure 2.

Historical Background

Summary: Curran Embankment, Cardiff

- 3.3 Cardiff (Caerdydd in Welsh) began life as a Roman fort ('caer' meaning fort or castle). According to Lewis (1849), Norman invaders under Robert FitzHamon, a relative of the Conqueror, seized control of the Welsh lowlands of Glamorgan, and he constructed a stone castle at Cardiff in the 1090s. The town of Cardiff grew up around it. Robert, Earl of Gloucester, a son of King Henry I, became lord of Glamorgan through his marriage to Robert FitzHamon's daughter Mabel. In 1147, the earl established the Benedictine priory of St Mary that stood at the south end of St Mary Street. King Henry II stopped at Cardiff on his way to invade Ireland in 1172, while his son, Prince John, acquired the lordship of Glamorgan when he married Isabel, Countess of Gloucester in 1176. By the end of the 13th century, Cardiff had grown to become the largest town in Wales. Weekly markets were held while its port was authorised to trade in wool and leather. Owain Glyndwr besieged and temporarily captured the walled market town in 1404 during the Welsh Revolt/War of Independence. He made himself master of the castle and carried off treasure that had been deposited there for safekeeping.
- 3.4 The shire of Glamorgan was created in the 1530s following the Act of Union between England and Wales. St Mary's Priory was dissolved by Henry VIII, and the town was divided between the parishes of St John in the north and St Mary comprising the southern part of the town and Cardiff Bay. The flooding of the River Taff washed away St Mary's Church in 1607, and it was not rebuilt until the 1840s. King James I granted the town a royal charter in 1608 establishing Cardiff as a borough. The fortress was garrisoned for the king during the Civil War, and Charles I spent several nights at the castle in August 1645. By the 18th century, Cardiff was still a relative backwater with a population in 1800 of around 2,000 inhabitants. The opening of the Glamorganshire Canal in 1798 and the Taff Valley Railway in 1841 improved communication with the rest of Britain, though it was Cardiff's access to the sea that would turn its fortunes and transform it into a world-renowned city.
- 3.5 The development of the city in the 19th century fell largely within the purview of one aristocratic family, the Crichton-Stuarts, Marquesses of Bute. The land to the south of Cardiff between the town centre and the sea formed part of Lord Bute's vast estates in Wales. The area traditionally had been agricultural land, but according to Davies (1981) the idea for a new dock had been mooted in 1822. The neglected Bute estate was then embroiled in lawsuits, but by the 1830s, Lord Bute was able to redirect his attention to the dock development project that was planned to rival Liverpool. The dock would provide a convenient shipping outlet for coal from the Bute mineral estates in the Welsh valleys. Work began on improving the Glamorganshire Canal and constructing feeder channels to the port, which passed just to the east of the study area, then the main development began in earnest with the construction of the West Bute Dock during 1834-39. Despite initial financial difficulties, demand for space grew quickly and as many as 160 vessels could be spotted at the docks at a time. In 1856-58, the Bute estate expanded the docks with a new facility of forty-six acres providing nearly two miles of quays and bringing overall capacity of the docks at Cardiff to two million tons of goods per year (Davies, 1981).
- 3.6 The second half of the 19th century saw not just the expansion of the docks, but also the construction of a huge industrial estate with factories, workshops, and warehouses. This development also required new areas of housing for the thousands of dock and factory workers. Though much of this housing went up in Grangetown on the west bank of the River Taff, there were also new terraces built along Bute Road, between the study area and the West Bute Dock, as shown on the 19th century OS plans of the city. This whole area became known as Butetown. Meanwhile the old part of Cardiff was reworked into a grand Victorian metropolis.

- 3.7 As demand for coal continued to grow during the latter part of the 19th century, the Bute docks became the largest coal port in the world. Every bit of space between the River Taff and the sea was claimed for development, but the Bute Company was running out of land, so they began reclaiming land from the foreshore. The Queen Alexandra Dock, built on reclaimed land at the southern end of the peninsula, opened in 1907 providing accommodation for larger, modern vessels. Exports reached their peak in 1913 when more than thirteen million tons of coal left Cardiff docks (Davies, 1981). Despite rapid growth and development during the 19th century, generating huge wealth for the Crichton-Stuarts, during the early 20th century, the city's fortunes began to decline as other ports competed to carry Welsh coal to markets around the globe. Cardiff nonetheless remained an important industrial centre right through the Second World War and post-war period.
- 3.8 Cardiff was granted city status in 1905, and in the 1950s it beat out several rivals to be designated the capital of Wales. This took on greater significance at the end of the 20th century, when the process of devolution divided the governance of Wales between the UK parliament and the Welsh National Assembly, housed on a site in Cardiff Bay less than a mile south of the study area.
- 3.9 In 1844 according to the tithe map and apportionment, the study area was still undeveloped salt marsh that Lord Bute had leased to Thomas Rees for pasturing. Rees also held several more parcels of meadow and pasture in Bute estate land on the opposite bank of the Taff. The first wooden bridge across the river was constructed in 1850 just to the north of the study area, and this encouraged the growth of the suburbs at Grangetown and Penarth. A decade later, Cardiff's ragged school, which educated destitute children in the town, petitioned the Admiralty for an old ship so the school committee could open an industrial school where juvenile vagrants would receive an education (Higginbotham, 2020). HMS Havannah was donated on condition that the committee paid for the ship to be towed from Plymouth to Cardiff. She was a fifth rate frigate fitted with thirty-six guns that had been launched at Liverpool in 1811 and saw service during the Napoleonic Wars (Colledge, 2003). Havannah underwent conversion in East Bute Dock and was later moved to a position on the moor at the south side of the Penarth bridge. Here the ship was floated by the spring tides (Carradice, 2015), and the movement repeatedly broke the ship's connection to corporation gas and water supplies, so eventually an embankment was erected around it, as shown on the 1880 OS plan.
- 3.10 Havannah was officially certificated as an industrial school ship, the first of its kind, in December 1861. There was accommodation for ninety boys aged between ten and fourteen who were taught nautical skills should they desire a career in the navy as well as other skills to prepare them for gainful employment with local tradesmen (Higginbotham, 2020). By 1900 more than 600 boys had been educated on the hulk, and as noted in the local press ('Havannah Industrial School, Cardiff Annual Report', South Wales Daily News, 16 May 1896) the annual report of the industrial school for 1896 estimated that at least eighty-five percent of these had "turned out well" and become law-abiding citizens. Despite this favourable assessment, the school was closed down in 1904, and the ship was broken up the following year (Colledge, 2003).
- 3.11 In 1878, an application was made to Parliament by the proprietors of the Glamorganshire Canal Navigation for a bill to authorise the company to construct a dock, basins, graving dock, quays, jetties, wharves and warehouses on the east side of the Glamorganshire Canal and on the west side between the canal and the River Taff ('In Parliament – Session 1878', The London Gazette, 20 November 1878). The bill would also authorise the company to construct a timber float or pond on the east side of the Dumballs Road to the north of the timber yard shown on the 1880 OS plan. This pond, however, was not sufficient and a second much larger pond was excavated on the west side of Dumballs Road at some point between 1880 and 1900. Here importers could store timber in water to prevent it seasoning before it could be processed at the neighbouring sawmill or sold on. As well as the two ponds belonging to the canal company, there was another timber pond adjacent to the Bute East Dock (shown on the 1900 OS plan) though this was only about half the size of the pond beside the river. The larger pond was filled in during the late 1930s to make way for a series of factories and engineering works each furnished with private sidings that connected them to the Cardiff Riverside Branch of the Great Western Railway.
- 3.12 Probably the best-known occupant of the site was Curran Steels. Edward Curran had founded an engineering firm at Hurman Road in Butetown in the early 1900s ("The girls were as strong as men at armaments firm Currans", Wales Online, 28 February 2009). Curran produced munitions in the 1930s, helping to prepare Britain for the Second World War. The new works at Curran Road was opened in 1942 and included a 100,000 square foot steel foundry with laboratory, dressing shops and pattern shops, and a promotional article from 1959 ('Company in Wales Offers Facilities', Foreign Commerce Weekly, 18 May 1959) stated that Curran Steels Ltd. was then manufacturing earthmoving equipment, industrial presses, castings and general engineering products at the works on Curran Road.

Heritage Assets

- 3.13 The following designated historic assets are located within the 1km study area: five Conservation Areas, two Grade I Listed Buildings, ten Grade II* Listed Buildings, 207 Grade II Listed Buildings and one Registered Park and Garden. The majority of these assets will have their significance unaffected by the tall building element of the proposals, but particular consideration needs to be given to buildings of landmark status, the contribution made by setting to the character and appearance of the St Mary Street, Mount Stuart Square and Pierhead Conservation Areas and the setting of the Grade II Registered Park and Garden of Granges Gardens to the south-west of the study site. Although it lies outside of the 1km study area, this assessment will also consider Cardiff Castle on account of its extremely high heritage status and landmark character.

Listed Buildings

- 3.14 The Grade I buildings of Cardiff Castle, Church of St John the Baptist and Pier Head building, and the Grade II* listed buildings of Cardiff Bay Station and Empire House will be assessed separately on account of their landmark status, grade and location in respect of views of the development site. The Grade II listed churches of Parish Church of St Mary the Virgin and St Stephen the Martyr and Parish Church of St Paul, which lie outside of any conservation area will be assessed separately on account of their historic function and landmark status, and the Grade II listed Central Station (and its associated designated buildings) will also be assessed separately owing to the open views towards the study site available from the southern entrance to the asset. Also separately assessed, on account of its memorial status and commemorative setting is the Grade II listed Statue of the Second Marquess of Bute. The seven Grade II buildings comprising warehousing and bridges associated with the docks will be assessed as a group. All other Grade II* and Grade II listed buildings located within the three conservation areas under assessment will be considered within their respective conservation areas.

Conservation Areas

- 3.15 The study site lies between the three conservation areas of St Mary's Street, Mount Stuart Square and Pierhead, with views across the study site available in relation to all three designated areas. Whilst Cardiff Castle is included within the assessment, the conservation area in which it is located is not included for separate assessment although a discussion will be had in regard to its contribution to the Castle's significance.

Non-Designated Heritage Assets

- 3.16 As detailed in the DBA (Orion 2020), the existing mid-twentieth century Curran's Steel buildings have a very low level of heritage significance, and are not currently identified by the Local Planning Authority in adopted planning documentation as being of local significance. A summary assessment of the buildings in relation to the proposals will be included for completeness.

4.0 Proposed Development and Potential Impact on Heritage Assets

Background

- 4.1 This section identifies and assesses the impacts of the proposal on the significance of the heritage assets located within and in the vicinity of the subject site. In assessing the heritage impacts of the proposal, the relevant policies cited in section 2.0 have been referenced.

The Proposed Development

- 4.2 The Proposed Scheme comprises Demolition of existing buildings and structures and the comprehensive mixed-use redevelopment of land at Curran Embankment to provide up to 2,500 new homes (Class C3), Business space (Class B1) and a mix of complementary leisure, food and drink, hospitality and retail uses (Class A1, A2, A3, C1 and D2); creation of new open space (including a new riverside park and water taxi stop); new pedestrian, cycle and vehicular access points; pedestrian footbridge; vehicular and cycle parking facilities; landscaping; public realm and other associated ancillary works. All matters reserved other than vehicular access

Potential Impacts on Heritage Assets

- 4.3 As noted in paragraphs 3.14 to 3.16, there are a number of listed buildings and conservation areas and a Registered Park and Garden located in the immediate and wider vicinity of the subject site. The proposed development, therefore, has the potential to impact on the settings and significance of the heritage assets identified.
- 4.4 In order to understand how any new development could affect the significance of these heritage assets, it is important to understand the specific heritage values which combine to inform that significance. An understanding of the contribution setting makes to its significance is also considered.

Listed Buildings in the vicinity

Cardiff Castle

Description and setting

- 4.5 This Grade I listed building (13662) is located c. 1.5km north of the study site. The listing description for the asset is as follows:

History

A Roman timber fort stood on the site from circa AD55-60, successively rebuilt, until around 280 a large stone fort was built, whose foundations define the present castle enclosure. Around 1081, William I built a motte within the Roman enclosure, upon which the present polygonal stone keep was built about 50 years later. This was modified c1300 with addition of S tower; an inner bailey wall was constructed, and probably the Black Tower. Around 1429-39, Richard Beauchamp, Earl of Warwick built a polygonal tower and new domestic range against W wall The range was enlarged in the late C16 by Henry Herbert, Second Earl of Pembroke. The castle was inherited by Lord Mountstuart, later the first Marquess of Bute in 1776 who had the western apartments modified by Henry Holland, and the grounds landscaped by Lancelot Brown. Further work 1817. Some work to Black Tower by John Prichard, 1850. From 1868, the Third Marquess of Bute employed William Burges (1827-1881), amongst the most important Gothic revival architects and designers of the period, to transform the castle into a High Victorian medieval fantasy with particularly exotic interiors, executed by a team of leading artists and craftsmen. The castle also retains important fittings and furnishings from this period. Burges' sources range across much of European Gothic art; the Arab Room introduces an Islamic influence. Burges' schemes were continued after his death by William Frame, although Burges' elaborate Grand Staircase, partially completed, was removed in early C20. The S gate and barbican Tower by J P Grant, 1920s; West gate 1921. The Roman walls were exposed and rebuilt to more than their original height between 1891 and 1923. The Castle was given to the City in 1947 by the 5th Marquess of Bute.

Exterior

Castle entered from S at Black tower (polygonal turret to rear) and S gate (rebuilt 1920s) with towers to R. South walls to W of Black Tower reconstructed in Medieval style by Burges with battlements with flaps between merlons, and slate roof. Curtain walls to SE, E and N reconstructed in Roman Style with crenelations, polygonal turrets, and enclosed wall-walk with arched openings. North wall has central gate with turret to each side; course of pink stone separates Roman Work from reconstruction. To N of N gate, wooden bridge on concrete piers in Roman style. On W side of enclosure, stretch of medieval curtain wall with C20 turret. West front of apartments has, from N, Bute Tower (raised 2 storeys by Burges) statues in arcading, tall square turret to L. Polygonal Beauchamp tower with machicolations and wooden (C19) fleche. Below, reconstructed West Gate (1920s). Square Herbert Tower (raised 2 storeys by Burges) with steep slate roof. Guest tower: gable end of tank tower. At SW corner, Clock tower, machicolated, with heraldic shields, polychrome statues, gilt clock faces, pyramidal lead roof of 2 stages with clerestory windows of Smoking Room; stair turret to N. In bailey, Motte surrounded by moat re-excavated in C19. Stone keep dodecagonal in plan; lias w freestone angles; S tower with turret (C20); stairs with base of turret enclosing well. Former curtain wall marked with modern masonry. Rear of S curtain with wooden walkway and lead? gargoyles; walkway continues round Clock Tower. E front of Western apartments has then tank tower with steeply-pitched slate roof to gallery. Then tall guest tower with tall polygonal turret to R. Steps up to Entrance to L of Beauchamp range with 5 polygonal turrets (but S turret to R of doorway 1927). North block (Holland) of 3 windows plus one set back Gothic fenestration (C20).

Interior

The interior has 17 rooms to the designs of Burges, and others show his guidance. Many of the rooms are of unrivalled sumptuousness and fantasy. CLOCK TOWER has rib-vaulted Winter Smoking room with decoration on the theme of Time. Above this, the Bachelor bedroom with theme of mineral wealth, bathroom with Penarth alabaster. The elaborate Summer smoking room with gallery, ribbed and painted dome, tiled floor, painted wall tiles. GUEST TOWER has Nursery with painted tiles of fables and nursery rhymes, overmantel with figures of Fame and Aesop's fox fables. Walnut Room with overmantel of Jack and the Beanstalk. ARAB ROOM with Islamic vault, trellis work, multi-coloured marble floor; marble walls, Arabic stained glass. BANQUETING HALL has open timber roof with fan vaulting and angels. Wooden screen to S. Chimneypiece in form of castle with equestrian figure of Robert, Earl of Gloucester. Wall-paintings of his exploits. OCTAGON STAIR in Beauchamp Tower; stone newel and Mouchette balustrade, paintings of Aesop's fables. Small vaulted ORATORY in French Gothic style. LIBRARY has paired Gothic doorways with relief monkeys; Gothic bookcases; chimneypiece with 5 seated figures representing alphabets and Third Marquess. ENTRANCE HALL (1930). LORD BUTE'S STUDY has lierne vault with foliage bosses and painted birds; panelled walls. Nearby, Office and NOAH ROOM. DRAWING ROOM in simple classical style. DINING ROOM uses scenes from life of Abraham to illustrate hospitality. Heavy painted ceiling with cylindrical beams. Overmantel with sculpted figures of Abraham and Sarah flanking 3 angels over Greek inscription. Stained glass with scenes from life of Abraham; carved and inlaid shutters. LADY BUTE'S APPARTMENT include panelled bedroom, and bathroom with stained glass roundels. CHAUCER ROOM decorated with scenes from Chaucer; octagonal plan top lit from fleche of Beauchamp Tower. Lantern has 2 levels of stained glass with figures from Canterbury Tales, and scenes from Tales, and Chaucerian subjects in tracery glass. Stencilled timbers to lantern. Crocketed arches with murals of Legend of Good Women (corbels of female figures). Walnut panelling inlaid with mother of pearl. Marble chimneypiece with female figures hanging heraldic shield on tree. Tiled floor with vine-leaf maze. Above dining room, LORD BUTE'S SITTING ROOM decorated with scenes of life of St Blane (Patron Saint of Isle of Bute); on door, painting of Seven Deadly Sins overcome by Virtue. Brass overmantel with sculpted beavers on lintel. LORD BUTE'S BEDROOM has ceiling with heavy beams on stone corbels supported by green marble shafts; 3 arches to each wall; beyond bedroom, bathroom. The ROOF GARDEN has marble-faced court with bronze columns supporting tiled roof; mosaic floor. Combination of Medieval and Pompeian styles; wall tiles, and Hebrew inscriptions; bronze fountain and statue of Virgin and Child.

Reason for designation

Listed Grade I as an exceptional building with outstanding architectural features from many historical periods. The High Victorian interiors are of the highest architectural and historic interest, and include important original fittings and furnishings, designed and executed by leading designers and craftsmen. Scheduled Ancient Monument. Cadw Ref No 5/0938/GM171(CDF)

- 4.6 Cardiff Castle is located within the Cathays Park Conservation Area and also within the Grade I Registered Park and Garden of Cardiff Castle and Bute Park, with both of these designations generating high levels of illustrative and architectural value on account of the extremely high quality historic built fabric present and the quality of the landscaping. To the south, the Castle grounds abut the St Marys Street Conservation Area, with views of the castle available along High Street, with its

presence still discernible at the junction with St Mary Street and Church Street, where again the high quality of the historic fabric generates considerable illustrative and architectural value. Owing to the curving nature of the thoroughfare, however, there are no views outward toward the study site in which the Castle is experienced in return. Long distance views across Cardiff are available from the tower of the twelfth century keep with historic and twentieth and twenty-first century towers prominent as skyline features (Plate 10). In its present state, the study site is not experienced in these long distance views.

Impact of Development Proposal

- 4.7 The 30 storey elements of the proposals will be experienced within the views available from Cardiff Castle keep but, given the distances involved and the presence of BT Stadium House within the view line of the study site from the Castle, the towers will not be a dominant feature within the skyline. This is evidenced by the TVIA montage produced by Axis and Mason for Viewpoint 16. The key visual relationship in terms of historic context is the legibility of the tower of the Church of St John the Baptist within the Castle environs, and the proposals will not detract from the experience of this relationship, as they will be less prominent in the environs than existing structures, particularly the twenty-three storey tower of the Radisson Blu Hotel at Meridian Gate. The location of the study site and the siting and orientation of the tower elements within it is such that there will be no coalescence of form with existing tall development in southerly views from the Castle in the direction of the Church. It is therefore considered that, while the proposals generate change within the setting of the asset, they do not harm the key contribution of the asset's setting, and in consequence the significance of the Castle is considered to be preserved.

Church of St John the Baptist

Description and setting

- 4.8 This Grade 1 listed building (13674) is located c. 1km north of the study site, and is described as follows in the listing:

History

Originally a chapel of ease of St Mary's Church, the present church originates mainly from later C15 with tower of circa 1490 (perhaps by William Hort); S arcade of chancel may be late C13. Tower said to have been commissioned by Anne, wife of Richard III. The medieval plan was a nave of 5 bays with 2-bay clerestoried chancel with separately gabled chapels, aisles, and tower. Church restored early C19, again in 1853; between 1889 and 1891 scheme of restoration by Kempson & Fowler replaced wooden gallery by outer aisles, lengthened and heightened the chancel, added vestries, and refaced much of building in Sweldon stone. Victorian and later embellishments include sculpture by Goscombe John and Comper; stained glass by Morris, Ford Madox Brown, Burne-Jones. Uncompromising 2-storey concrete vestry inserted in S aisle by George Pace, architect, 1975.

Exterior

Older parts of church in blue lias with Dundry stone dressings; Victorian extensions and refacing in Sweldon stone with bathstone dressings. Mainly Perpendicular tracery. Nave of 5 bays, chancel, west tower (tower porch), double aisles to N and S, S porch, shallow N porch. Tower of 4 stages with stepped diagonal buttresses; low ground floor stage is porch open on 3 sides, doorways with multi-moulded arches; rib vault to porch. Above entrance, W side has 5-light Perpendicular window; above this, in middle stage, tall window with pierced diaper and Perpendicular 2-light head; above this, bell stage has 2-light window with pierced stonework. Tower crowned by magnificent open work arcaded and battlemented parapet with openwork corner pinnacles and small gargoyles. Chancel has clerestorey; polygonal turret in angle with N chapel; 5-light Perpendicular E window. South West vestry with tall chimneys. South side has 3 and 4 light square-headed windows with frieze of flowers and animals over; blocked doorway in angle with aisles.

Interior

Nave has Perpendicular arcade with lozenge-shaped piers and 2-centred arches. Lower arcades to Victorian outer aisles. High and wide chancel arch. Four bay chancel with clerestorey. Chancel roof on Victorian head-corbels. Chancel S arcade with circular shafts (circa 1300?), N arcade similar to nave; reredos by Kempson & Fowler has sculpture by Goscombe John (1891). To N of chancel, Herbert chapel with wooden screen (part C16, part C17. Herbert Monument (early C17) with effigies of knight and lawyer, and relief of 4 cherubs. Reredos in S chapel by Comper (early C20). Tower arch, stone steps up to platform over porch; C19 iron railings. The church retains good C19 stained glass

including N outer aisle windows by Morris & Co (apostles by Morris, Abraham & Noah by Ford Madox Brown, Melchisedek by Burne-Jones). Other glass by Dixon, and Belham & Co (to designs of JP Seddon). Several Neoclassical tablets relocated to tower. Uncompromising 2-storey concrete vestry inserted in S aisle.

Reason for designation

Graded I as the historic parish church of Cardiff, the main remains outside the Castle of Medieval Cardiff, with especially fine medieval tower and important monuments and good C19/early C20 fittings and glass.

- 4.9 Located to the north of its roughly rectangular churchyard plot, the Church of St John the Baptist is experienced in an urban setting with buildings of high architectural calibre in its immediate environs listed at Grade II* and Grade II. This setting context endows the asset with high levels of historic illustrative and architectural value. In its wider environs the asset is experienced through glimpsed and partial views of its elaborate tower, with these views primarily available along Trinity Street, Church Street, St John Street and Working Street, but also St Marys Street, where the framed view along Church Street is particularly striking (Plate 11). At ground level the high density of built form and the building heights of the properties flanking the thoroughfares occlude any views of the asset from the southern end of the St Marys Street Conservation Area, but the tower is visible in views from the Castle keep, although experienced in the context of later tall buildings, particularly the Radisson Blu hotel at Meridian Gate, which challenge its historic skyline dominance. In its present state the study site is not experienced within these wider views, as previously stated with regard to the Castle.

Impact of Development Proposal

- 4.10 As stated above at paragraph 4.12, the location and siting of the tower elements within the development is such that there will be no coalescence with existing tall structures, and the distance from the Church is such that the scale of the building will be significantly reduced in its experience within the city skyline. The proposals will not affect the experienced relationship between Church and Castle in long views from the Keep, and as such will conserve this element of the Church's significance. There is no direct intervisibility with the study site from the asset, and it is considered that, on balance, the significance of the asset is not harmed.

Pierhead Building

Description and setting

- 4.11 This Grade I listed building (14055) is located c. 1km south-east of the study site and is described as follows in its listing:

History

Built in 1896 by William Frame, assistant to William Burges as offices for the Cardiff Railway Company, successor to the Bute Dock Company. Medievalist style with strong influence of Burges (in contrast to the Classical dock offices at Barry) combining muscular Gothic and French Renaissance elements. 2-storeys; brightly constructed throughout in glazed terracotta, representing a fine example of the use of this material; abundant sculptural ornament and banding. Slate roofs; small-pane glazing, round headed to 1st floor and square headed below.

Exterior

Design dominated by 2-stage clock tower at S end over main entrance; pyramidal roof and crenelated parapet with gargoyles. Clock faces recessed beneath semi-circular arches with fleuron panelled balconies; flanking lions heads and coats of arms. Splayed oriel over main entrance including crenelated transom and machicolated bracket base. Squat round arched entrance with deeply rounded jambs; battered bases to flanking octagonal and domed turrets. Panelled doors, half-glazed tympanum and swirling bands of foliage. Larger, Low Countries style, polygonal corner towers beyond, incorporated into main rooms and glazed to each face; pyramidal roof, gargoyles, colonnettes and foliage band between floors. 7-bay left hand side, the southernmost of which is taken up with exceptional chimney breast, heavily enriched with terracotta ornament including steam train and ship over the company's motto 'Wrth ddwr a than'; the whole set in a tiered frontispiece-like frame and topped by 3-linked chimney stacks; bronze commemorative plaque to base. Central 3-bays are divided by buttresses with polygonal faces and domed caps. Steep-hipped roof tower beyond over entrance inscribed Bute Docks Co. ; round arched with panelled surround;

chimney stack and band of narrow round arched panels to top. Northernmost bay has bracketed gable-oriel with 3-light transomed window; gable has blind oculus and finials. Simpler right hand side with central 3-bays divided off as before; 2-windows near southern end are set in foliage surround. Plain N end.

Interior

Entrance is onto square lobby, decoratively tiled and with panelled ceiling; round arched recesses to both sides with labels and nook shafts. This leads through to tall and grand hall; includes terrazzo floor, with central roundel repeating the company's motto and panelled ceiling. Church-like, double-arcaded hall of offices to rear through full height, moulded arch with foliage spandrels; terracotta detail includes twin pilasters between each arch, rising to carry the glazed clerestory roof along the central nave. The main stairwell leads off the front right hand corner of the entrance hall. The staircase is enriched by varied and colourful materials including granite treads, terracotta (stellar-section) balusters including enormous newels and green-glazed tile handrail; gilded Minton style tilework to dado (ca 1.8m high) - includes swagged band to top. The single-best room is the Port Manager's office on 1st floor with castellated and canopied 'medieval' chimneypiece with heavily foliated columns and herringbone tiled fireback; panelled ceiling with pendant to octagonal centrepiece. Round arch into corner tower bay in this office and that below. Crenelated and half-glazed partitions to 1st floor office corridors; panelled doors. Original ironwork spiral stairs inside clock-tower manufactured by St Pancras Ironwork Co, London; curved braces to treads.

Reason for designation

Listed Grade I as a central and especially important building to the historical and visual dockscape of Cardiff; it is furthermore an exceptional Victorian building reflecting the confidence of the period in its flamboyant architectural detail, particularly in its use of terracotta.

- 4.12 Located within the Pierhead conservation Area to the south-east of the study site, the asset's setting relates primarily to the docklands landscape and the numerous designated building within its immediate and wider vicinity, including those visible within the adjacent Mount Stuart Square Conservation Area. The historic fabric in its environs endows the setting of the asset with considerable illustrative value, whilst the sensitive re-development of the area, and the high quality of the twentieth and twenty-first century development in its vicinity serves to enhance the sense of place, through careful siting, scale and design (Plate 12). A key view exists to the north-west towards the Mount Stuart Square Conservation Area which is particularly sensitive, not only in regard to the setting of the highly designated asset, but also the conservation area in which it is located, and that of the neighbouring conservation area (Plate 13). Long views of the Pierhead Building are also obtainable from across the bay at the Norwegian Church (Plate 14) and various other points. The study site in its present state does not feature within this view, and currently and therefore makes a neutral contribution to the setting of the asset, with no bearing on its significance.

Impact of Development Proposal

- 4.13 The thirty-storey tower elements of the development proposals are likely to be experienced in the north-westerly view from the asset, will constitute a change to the built fabric of the wider environs and may potentially compromise the legibility of the historic skyline of the Pierhead Conservation Area as it is currently experienced. The historic buildings within the Mount Stuart Square Conservation Area, which are visible from within the environs of the Church will screen any views of the towers from within the Pierhead Conservation area owing to their height, and therefore the integrity of this historic contextual relationship will remain intact. Views out from the Pierhead across the bay toward key buildings will not be impacted, although the towers will be experienced in return views from locations such as the Norwegian Church (Plates 27 and 28). In these views the distances involved, the presence of twentieth and twenty-first century development to the waterfront, and the striking terracotta fabric of the Pierhead building will mean that the towers will be subservient to the landmark building in this view, and will not form any coalescence with any approved works in terms of cumulative impact. The modelling in plates 27 and 28 does not include for topographic variations, but as Cardiff Bay is generally flat, the images produced generate a reasonable level of certitude with regard to visual impact. On balance, it is considered that whilst the proposals generate a small degree of change within the environs of the Pierhead Building, its landmark status remains unchallenged, and as a consequence the significance of the building is preserved from harm.

Cardiff Bay Station

Description and setting

- 4.14 This Grade II* listed building (13963) is located c. 614m south-east of the study site, and is described as follows in its listing details:

History

Built as offices for the Taff Vale Railway, and first used in 1843, but southern section apparently an addition of 1860's. Following construction of new offices at Queen St in 1862, the building was let as consulates (and known as 'Consulate Chambers'), but with waiting room and ticket office in southern section. Adapted as station in early C20, following closure of old Cardiff Docks Station, formerly on the E side of the tracks.

Exterior

Stuccoed walls. Hipped slate roof with deeply overhanging eaves. Tall stuccoed chimneys with classicising cornices, northernmost 2 chimneys at right angles to road, others parallel. Bute Street facade in two sections, northern of 3 storeys and 4 bays, articulated by banding and pilaster strips. Second floor has 4 almost square windows set close to eaves, central pair are grouped closer. First floor windows are set deeply with panelled aprons below. Band course at this level continues round building. On ground floor, windows follow line of those above, and have stone sills but northernmost is round headed. To left (N) of this window is round headed doorway with architrave and keystone. Southern section of storey has group of three large tall windows (small pane sashes) with moulded architrave supported on brackets and bracketed cornice above. On ground floor 3 windows grouped together, central window blocked, R window without architrave (apparently blocked doorway). To L of windows, round-headed doorway similar to that at N end of facade. Elevation facing S has two storey semi-hexagonal bay with pierced parapet above. Each face of bay has window on each floor, each surrounded by moulded architrave on small brackets. To W, elevation somewhat altered and obscured by station canopy (modern) but follows pattern of E facade, omitting large boardroom windows. Station platform with free standing cast iron columns in Italian Renaissance style probably relocated. To N, elevation has doubled pilaster strips at ends, but door and window openings altered for fire escape.

Interior

Ground floor altered for use as Railway Museum.

Reason for designation

Graded II* as an exceptionally early surviving example of purpose built railway architecture in Wales.

- 4.15 The Cardiff Bay Station is located to the northern extremity of the Mount Stuart Square Conservation Area and forms a landmark gateway structure within the designated area. Views northward and southward contribute illustrative value in the form of historic context and also architectural and aesthetic value. The study site is not presently experienced in relation to the asset and does not form part of its current setting.

Impact of Development Proposal

- 4.16 The thirty-storey tower elements of the proposed development will be experienced in views of Cardiff Bay Station from Hemingway Road, a view which is identified as a key view within the Mount Stuart Square Conservation Area (Plates 15, 29 and 30). In these views the vertical stacks of the Station are prominent against the skyline, and this quality is likely to be marginally compromised by the presence of the towers. This having been said, as the asset's significance is derived primarily from its early date and purpose built construction, and its landmark quality is of lesser contribution to its significance than for a building such as a Church, where visual dominance of the landscape/townscape very much contributes to the significance of the structure. The presence of the towers would not impact on the legibility of the building with respect to the railway line, and the legibility of the historic context with regard to historic buildings in the vicinity of the station will be retained (Plate 16). On balance therefore it is considered that whilst the proposals constitute a change within the environs of Cardiff Bay Station, they do not impair the legibility of the asset in contextual views which generate illustrative value, and the significance of the asset is thereby preserved.

Empire House

Description and setting

- 4.17 This Grade II* listed building (14007) is located c. 600m south-east of the study site and is described as follows in its listing:

History

Built 1926 by Ivor Jones and Percy Thomas, architects of Cardiff; contractors - Messrs William T Nicholls of Gloucester. The scheme was conceived before the war by Commander Charles E Evans, Chairman of the Company, which was one of the largest firms of coal exporters (and later on shippers) in South Wales.

Exterior

Neo-Georgian facade to Hennebique type reinforced concrete structure with largely flat roof, except to the front where it is of mansard form. Symmetrical 7-bay front elevation, 5-storeys with 2-attic storeys above. Flemish bond red brick with channelled Portland stone ground floor and similar freestone dressings including bracket cornice to the top, bracket pedimented architraves to 1st floor and guilloche pattern ground floor cornice band; brick band course below 4th floor. Westmorland slate mansard roof to front with high parapets and end brick chimney stacks (cut down to left); the upper attic has flat-roofed dormers and the lower on has gabled dormers with pediments over the windows. Horned sash windows; 12-pane to the main floors with brick voussoirs to 2nd, 3rd and 4th floors and channelled voussoirs to ground floor; 9-pane to attic. The down pipes are contained in corner recesses. Central entrance with bracket cornice and applied lettering to frieze; traceried fanlight, with roundels to spandrels, a fluted band over renewed panelled double doors opening onto inner porch. The rear is utilitarian and almost entirely glazed with steel framed windows. The design includes two light-wells either side of the central hall; these are faced in pale coloured ceramic tiles. Semi-circular arched entrance from inner porch to hall with similar glazed and traceried fanlight including central urn over frieze carried on fluted pilasters in Palladian manner; latticed glazed double doors with fluted borders and brass.

Interior

The interior retains fine contemporary Classical detail. Entrance is onto a semi-circular arched tunnel vaulted cross-shaped hall, lined in fielded mahogany panelling and floored in black and white marble tiles; panelled bands around the arches. The front or entrance corridor part is coffered with inset bosses; the central foyer is groin vaulted with large and polygonal copper lamp ornamented with angle colonnettes; at the four corners of the foyer the panelling is advanced to imply that the vaulting is carried on crossing piers. The transeptal areas are top-lit from the light-wells with square-panel glazing; both have fine round-arched doorways with similar detail as before. At the rear of the hall is the simple staircase rising around the lift-shaft retaining original frame and solid mahogany car; manufactured by Waygood - Otis; clock face to front. The right hand hall doors open onto the former Accounts section with lobby and half-glazed partitions. Off this is the rectangular Board Room/Chairman's office; mahogany panelled up to a height of approx. 2.5 M some of the panels are in fact disguised cupboards; above are rectangular foliated borders around pictures; the ceiling has dentil cornice and finely detailed bands of festooned plasterwork forming a semi-circle in front of the chimneypiece - marble fireplace and mahogany bracketed mantelpiece; bracketed cornice to doorcase. Some of the original brass light fittings are retained.

Reason for designation

Graded II because of the especially finely detailed floor interior which has been well preserved and, together with the facade, represents the best South Wales example of inter-war, Neo-Georgian architecture; it also makes an important contribution to the commercial history of Butetown and Cardiff. Group value with other listed items on Mount Stuart Square.*

- 4.18 The setting of this highly listed building relates most strongly to the heritage assets within the conservation area, as attested by the group value element of its significance. Sited to the northern edge of the Mount Stuart Square Conservation Area, the building presents its rear façade to the north, which is experienced in the context of twentieth-century development in Butetown (Plate 17). This aspect of the asset's setting does not furnish any heritage value in terms of the asset's significance.

Impact of Development Proposal

- 4.19 Whilst the 30 storey towers will be experienced in views from Hemingway Road, Empire House is not experienced in this context, as the appreciation of its special interest is limited to views of its

principal façade in the context of the surrounding built environment of the historic fabric of Mount Stuart Square and the height of built form, together with the narrow nature of the thoroughfare generates a sense of enclosure which curtails outward views. As stated in the listing, the interior of the building is in great measure the reason for its designation, and the proposals have no impact on the experience of this aspect of the designated heritage asset. It is considered that the proposals therefore do not have any impact on the significance of this asset, and it is therefore preserved from harm.

Parish Church of St Mary the Virgin and St Stephen the Martyr

Description and setting

- 4.20 This Grade II listed building (13981) is located c. 370m to the north-east of the study site. It is described as follows in its listing:

History

Built in 1843 by John Foster, architect of Liverpool, in 'Round-arched Style'; cost £5,724. Replaces destroyed medieval church. Funded mainly by Second Marquis of Bute and funds also partly raised by a sonnet written by William Wordsworth - "When Severn's sweeping flood had overthrown St Mary's Church...". Enlarged with added vestries in 1907 by G E Halliday, Diocesan Architect in memory of Rev Griffith Arthur Jones. To preserve traditional liturgical orientation, the apparent main front of the church facing Bute Street is actually the rear of the church, with entrance front facing W.

Exterior

Rubble with freestone dressings and belfries; asbestos tile roofs. Twin towered, 4-window front to Bute Street, disguising the fact that the main entrance is in fact at the W end beside the Vicarage. Pyramidal fish scale caps to tower roofs with grotesque carved corbel table and narrow louvered belfry openings; pilaster buttresses. Central rose window with broadly scalloped band below and lower bands to the towers. Diamond leaded glazing with colonettes and scalloped capitals. Broad central entrance with intersecting chevron and hatched ornament to arch mouldings; plain tympanum and trumeau; twin doorways. Narrow arched recesses to either side. Similar chevron ornament to shouldered outer doorways. Aisle set back to left hand side. 1907 sneaked rubble vestry range to right, the gable end of which has crucifix finial and 3-light central window with granite columns and round headed lights. Parapet along right hand side with recessed panelling and small round arched lights. 5-bay side elevations with plain round arched fenestration and buttressing between bays. Lean-to aisles with similar detail. Continuous impost band. Westwerk-like elevation to W with square turrets and pinnacles; central rose window to gable with bracketed band to base. 2-tiers below of 3-round arched windows with sill bands and coloured glass. Octagonal stair turrets at ends of aisle. Tripartite entrance on 3 storeys composed of high central arch spanning twin main doorways and with lower flanking doorways; cushion capitals.

Interior

Nave and aisles oriented traditionally with altar and apse at E end despite towered apparent entrance front facing Bute Road. Present E end arrangements by J D Sedding, architect, 1884. Flat ceiling to five-bay nave. Round-headed clerestory windows with plain glass. Arcades with round-headed arches and massive cylindrical columns with Norman style capitals and bases. Hood moulds over arches with grotesque heads at points of intersection. Simple trusses to lean-to aisle roofs on stone corbels. Aisles formerly had galleries (removed 1883); round-headed windows with stained glass, painted rubble walls. At W end of nave, gallery over entrance lobby has organ brought from Clifton Street Presbyterian Church). Fine wrought-iron screen formerly chancel screen of St Dyfrig's Church, Cardiff) positioned between westmost columns of nave. Wooden pulpit and choir stalls in eastmost nave bay. Chancel arch flanked by paintings of St Winifred (L) and St Margaret (R). Chancel arch with engaged Romanesque shafts and imposts. Orders of arch with painted decoration. Elaborate apse with decoratively painted ribbed 5-bay vault. Apse wall arcade has Gothic arches and tall shafts with stiff leaf capitals. Painted medallions of Biblical scenes in C16 Italian style. Arcade subdivided into 5 groups of 3 gothic-arched niches, those flanking altar have life sized statues of the twelve apostles on plinths (by Searle, of Exeter, 1884 onwards). Lowest zone has blind arcading in Romanesque style. Above altar, elaborate free-standing altarpiece with Adoration of Shepherds in early C16 Venetian style, and carved frame with angels on end buttresses and symbols of Eucharist and Christ's passion. Altarpiece by P Westlake 1884. Small chapel at E end of S aisle with simple round-headed entrance arch.

Reason for designation

Listed as an interesting and well preserved example of this style, for its dominant location, and for its historic interest.

- 4.21 The Church is located within a rectangular plot, with the Primary School located to the immediate north-west, and the school playing field located to the south. The rear façade of the Church, designed as its principal façade, faces Bute Street, with the paired towers visible in views along the thoroughfare (Plate 18), particularly from the south. In all views the towers are experienced in the context of twentieth and twenty-first century development, with the twenty three storey Radisson Blu hotel located to the junction of Bute Street, Custom House Street and the A4160 present in views northward, and the pair of sixteen-storey towers at Loudoun Square evident in the southward views. The tree planting to the churchyard plot and Bute Street provides a welcome softening of the urban forms, and is a positive element visually within the setting. Adjacent to the Church of St Mary is the Grade II listed Greek Orthodox Church of St Nicholas (see below) which adds illustrative and architectural value to the setting and significance of the asset.

Impact of Development Proposal

- 4.22 The 30 storey tower elements of the proposals will be experienced in the context of the Church in views from Bute Street and Lloyd George Avenue, looking south-west. In views from the latter, the towers to the east front of the Church are glimpsed above the tree line, but their landmark quality is considerably reduced in comparison with views to the north from both this road and Bute Street. In closer views of the asset from Bute Street, where the particulars of the architectural detailing of the building are most evident, the proposed towers will be experienced in the background, particularly in winter when the trees bordering Bute Street are not in leaf, and the height of the proposals will generate a very minor degree of challenge to the landmark nature of the towers of the listed building. At present the 16 storey developments at Overstone Court are experienced in the context of the Church, but the distance means these elements are subservient in height and do not challenge the landmark quality of the designated asset. At potentially twice the height, and experienced from a closer distance, it is considered that the proposed towers will be less subservient in respect of the Church, however, in views where the architectural quality of the building is readily apparent the proposals' height will be subservient to that of the designated heritage asset. In many other views of the Church towers, the orientation and bulk of the Church itself prevent any experience of the proposed towers. It is therefore considered that, on balance, the landmark status of the asset will be conserved.

Parish Church of St Paul

Description and setting

- 4.23 This Grade II listed building is located c. 550m south-west of the study site and is described as follows in its listing description:

History

Designed 1888 by J P Seddon and J Coates Carter, architects, to replace iron church of 1879, on land donated by Lord Windsor who also gave £4000 towards cost of church. Nave and aisles built 1889-91; chancel 1901-2, at further expense to Lord Windsor of £3000. Projected tower and steeple never built. The church is interesting for the early use of concrete aggregate materials in its construction.

Exterior

Church in Geometrical Decorated style. Grey Pennant stone with dressings and banding of pink sandstone and Portland cement with pink pebble aggregate. Continuous tiled roof over nave and chancel. Nave, chancel, aisles, base of uncompleted tower. West front has 2 polygonal turrets with pyramidal roofs above arcading, niches for statues (not executed). Four-light Decorated window above gabled tripartite doorway with grey shafts to arches (gable flanked by flushwork). Side elevations of 5 cross-gabled bays with tall 2-light windows with quatrefoils, oculi in gables, lancets to E and W aisle returns. East window of 5 lights (mandorla above in gable apex). Chancel elevations of 3 bays, three 3-light windows to N, to S, two 3-light windows, and stump of uncompleted tower with has battered angle buttresses and tall arcaded panel. Vestry to S of chancel.

Interior

Lofty five bay nave with tall slender piers, 3-bay chancel at same height. Boarded roofs to nave and chancel of trefoil section, tie-beams and arch-braces on pointed corbels, boarded ceilings to aisles. Panelled wood fittings to chancel. Wooden pulpit with sculpted figures, and choir stalls by Coates Carter. Stained glass in E (war memorial) window; main theme is crucifixion, with, at base of window, panels of trench scene, artillery, seaplane, merchant ship; by Burlison & Grylls, 1920.

Reason for designation

Listed as well-designed church by well-known local architect.

- 4.24 The Church is located centrally to its small rectangular churchyard plot, with the St Paul's community centre (of late twentieth-century construction) immediately adjacent. The Church is flanked on all sides by residential development, and owing to the lack of a tower, the asset is not experienced in the wider environs. In the view along St Fagan's Street the sixteen-storey towers of the Overstone development, adjacent to the study site, are evident (Plate 19). The Church gains a degree of illustrative value from the contemporaneous residential buildings in its immediate vicinity and historic context from Grange Gardens, although there is no direct intervisibility with the latter.

Impact of Development Proposal

- 4.25 The proposed tower elements will be experienced in the context of the Church in views along St Fagan Street and Paget Street, particularly during the winter months when the tree planting to the churchyard lot is not in leaf. Whilst present, however, the proposals will not affect the ability to experience the design quality of the historic building and, given the absence of a tower, the asset's landmark status is much truncated and not a particular feature of its significance. Having regard to this the proposals are not considered to harm to the significance of the historic building.

Greek Orthodox Church of St Nicholas

Description and setting

- 4.26 This Grade II listed building (13986) is located c. 500m north-east of the study site and is described as follows in its listing description:

History

Built in 1906 by James and Morgan, architects of Cardiff, on a site previously occupied by the Bute Dock Brewery. Cardiff has had a long history of trade with Greece and as a result quite a large Greek community had settled here by the end of C19; much of the money to build this church was raised by contributions from Greek sailors (see foundation stone). The church was consecrated in 1919; restoration, especially to the dome, carried out in 1985. New copper roofs, 1991.

Exterior

Byzantine style, Greek Cross church with central lantern and saucer dome and apsidal E end; the plan includes an ambulatory and a narthex, lead roofs to the cupolas at the corners of the W front and modern felt roof to the rebuilt and tile hung dome; brick chimney stack. Mostly leaded small-pane glazing; modern 2-light dome windows. The central part of the gabled W front has segmental headed window with label and divided into 3-lights by giant order pilasters that flank the main entrance below; triangular headed tracery. 3 semi-circular headed doorways each with fish scale type ornament to recessed stone tympani; panelled doors. At the gable apex is a timber bracketed bellcote - bell dated 1988 to right of entrance. Set back to either side are low, polygonal, corner 'towers' with cupolas, tall round headed windows and toothed cornices; that to right contains gallery staircase. The side elevations have simple buttresses and round headed windows; advanced voussoir arches beneath dome; blocked doorway to N side. Stepped 3-window apse with toothed cornice; priests doorway to S. Steps to cellar on S side.

Interior

The interior is in similar style and is entered through the narthex. The main body of the church below the dome is separated from the sanctuary by the tripartite iconostasis with gated segmental arch to centre; marble effect to the dividing piers. There was originally painted decoration to the dome depicting the Pantocrator to centre and the four evangelists to the corners which are not of pendentive form. Gallery to W end (originally for the women) with grid pattern timber front. Pedimented altar against blocked doorway on N side; old seating alongside walls retained; Bishop's throne dated 1929. The sanctuary has tunnel vaulted roof and the flanking areas of the apse have flat ceiling. Some coloured glass.

Reason for designation

Listed for its special interest as a rare Welsh example of this style of architecture and for its historical importance to Cardiff.

- 4.27 The Greek Orthodox Church of St Nicholas is flanked immediately to the south by the Greek School, and is closely surrounded by residential development made of converted shipping containers and other twentieth-century development of two and three storeys in height. From Bute Street, the saucer dome and apsidal east end are visible from Bute Street in the context of the Church of St Mary, where the dominant form of the latter ecclesiastical building is the key element of setting that provides heritage context (Plate 20).

Impact of Development Proposal

- 4.28 As discussed above at paragraph 4.26, the proposed 30 storey towers will be experienced in views from Bute Street, in the context of this building and the Church of St Mary adjacent. Unlike the latter Church, however, the Greek Orthodox Church's significance is not derived from its landmark status, but rather its rarity in form. In this regard the presence of the towers in the wider environs is not therefore a direct challenge to the asset's significance. The significance of the asset is therefore considered to be preserved from harm.

Central Station Group

These five designated assets, all listed at Grade II, will be described separately, but treated collectively with regard to their settings.

Descriptions

Central Station

- 4.29 This Grade II listed building (20281 & 14063) is located c. 470m north of the study site and is described as follows in the listing (s) which are duplications:

History

Completely rebuilt 1932-1934 to designs of Great Western Railway architect's department. Replaced station of 1895-6. Rebuilding included changing level of station approach (formerly at platform level), new booking hall, platform buildings, extending platforms, new passenger subway, new goods depot, new signal boxes and signalling equipment, building 18 new bridges, widening and strengthening permanent way. Modernization of booking concourse late C20.

Exterior

Booking Hall in natural Portland Stone with Cornish granite plinth. Interior walls have granite bases. Devonshire green marble. Pilasters capped with Ashburton black marble. Ceramic tile facings. Platform buildings and subways in glazed blocks (Carrara ware by Doulton). Grey granite plinths to platform buildings. Portland stone Booking Hall in classical style with clock above slate roof. Elevation facing Central Square has end blocks each of 3 bays with parapet, cornice with central triangular pediment, Doric pilasters and Roman Bath type lunettes with keystones. Between this is canopy cantilevered on lattice girders and supported by wrought-iron tie bars, canopy continues on E return of building. On ground floor, inserted modern fascias to shops. Return to E (left) of three bays; parapet and cornice with pediment over central bay, plain frieze, 4 Doric pilasters, central bay very slightly advanced with blind round-headed arch with keystone.; beneath canopy, former entrance doorway blocked, with render scribed and painted to imitate Portland stone. Return to W (right) plain. Adjoining wing (W) in Portland stone on grey granite base; openings for former shops and public house; parapet has square openings with iron grilles. To E (left) of Booking Hall, long angled retaining wall, parapet with square openings with iron grilles. Wall panelled below to take advertisements. At R end of wall, adjoining Booking Hall, doorway to subway of luggage entrance to station. To rear, (S), blind panelling faces platforms. PLATFORMS: Three island platforms in stone but some extensions in brick, flooring originally in asphalt below canopies, and flagstones where not covered, but some concrete block replacement. All platforms have canopies with corrugated roof covering. Roof system of lattice girders with central double-pitched area and outer cantilevered canopies. Roof supported by slim cast-iron Tuscan columns, and on corbels in platform buildings. Cross girders of central areas with 3-centred arches; longitudinal girders to each bay with elliptical arches. Boarded wooden valences to roofs. At W end of platforms 3 & 4, roof divides (at former platform 5), and consists of 2 umbrella-type canopies supported on tapering I-beams. At uncovered ends of platforms, lamp standards in concrete. PLATFORMS 1&2: Two blocks of platform buildings, single storey, in cream glazed blocks (Carrara ware by Doulton), brown surrounds for advertising posters. Polished grey granite plinths. Inscriptions 'Cardiff' in raised brown ceramic lettering. Mullioned windows. Wooden doors with oval windows. Block to E contains toilets and buffet (modernised), block to W has offices and waiting room. Interior of waiting room with wood panelling and simple coved ceiling. Two housings to goods lifts with modern corrugated walls at platform level, lift housings project above canopies clad in wooden weatherboarding with hipped roofs. One wooden kiosk attached to goods lift; one free-standing kiosk between main blocks. Additional service block in brick to W of platform. PLATFORMS 3 & 4: Formerly Platforms 3,4 & 5; W end of platform formerly divided to give extra run-in for trains. Two blocks of platform buildings similar to those on platforms 1 & 2, but narrower. Eastern block has upper storey projecting above canopy (restored 1998 after fire), gabled ends, pilaster strips project above parapet; T-bar glazing to windows. Two housings to goods lifts with modern corrugated walls at platform level, lift housings project above canopies clad in wooden weatherboarding with hipped roofs. PLATFORMS 6 & 7: Two blocks of platform buildings

similar to those on other platforms. Western block extended to E, as buffet, and faced in cream ceramic tiles. Further building to W in concrete blocks painted to resemble Carrara Ware. Two housings to goods lifts as other platforms.

Interior

Booking Hall of 11 bays. Blind arcading to rear wall and above entrance. Five central bays have coved glazing ceiling, lit by artificial lighting. Three bays to each end have ceilings with elliptical vaults and are lit from front by tripartite Roman bath type lunette windows. Bays articulated by dark green marble giant Doric pilasters on granite bases. Modernisation includes insertion of enquiry office at E end, and shops to N. Passenger subway has walls faced with cream ceramic tiles with brown tiled surrounds for advertising posters. Two stairs lead to each platform. At foot of stairs to platforms, ceramic tile panels with raised cornice and surrounds, lettering, numerals and direction fingers in brown on cream background. Stairs to platforms also have walls in cream and brown ceramic tiles, wooden handrails; stairs to mainline platforms have central handrail on iron supports, stairs to branch lines narrower without central handrail. At platform level, stairways have iron rails in simple geometrical 1930s style.

Reason for designation

Listed as most complete example of GWR major city railway station of the period following the regrouping of the railway system.

Water Tower at Cardiff Central Station

- 4.30 This Grade II listed building (21705) is located c. 546m north of the study site and is described as follows in the listing:

History

Dates from major rebuilding of Cardiff Station in 1932-34; served the former Fish Platform.

Exterior

Six concrete ribs arranged in hexagon plan support, on brackets, cylindrical water tank. Brick infill between ribs; boarded door to SE. Smaller cylinder above tank with doorway. Interior not inspected

Reason for designation

Listed for group value with Cardiff Central Railway Station.

Premises occupied by Jacobs Antique Market

- 4.31 This Grade II listed building (14025) is located c. 470m north of the study site and is described as follows in the listing:

History

Probably early C19 house (appears to be on O'Rouke's plan of 1849), marked as inn on early C20 Ordnance Survey maps and situated on what was formerly wharf of Glamorganshire Canal.

Exterior

Two storeys, three windows wide. Slate roof and red brick chimney. Smooth render with pilaster strips and band course between storeys. First floor windows small-pane hornless sashes, end windows 8 panes over 8, central window opening the same size but fitted with narrower sash with 6 panes over 6 and flanking panelled infill. Ground floor windows small-paned horned sashes. Central entrance doorway has triangular pedimented doorcase with fluted pilasters. Panelled reveals to doorway. Interior not inspected

Reason for designation

Listed as rare surviving building of this period in central Cardiff.

Central Workshops of City of Cardiff Operational Services

- 4.32 This Grade II listed building (19090) is located c. 618m north of the study site and is described as follows in the listing:

History

Built circa 1902 as Clare Road Depot for Cardiff Tram Service. Contractors Messrs D W Davies. In 1923 some alterations to allow trams to enter from S instead of N. Extensions and alterations by S C Taverner of Newport in 1925. Converted to trolleybus depot 1942, closed to trams 25 August 1946.

The depot served routes serving W of City; E routes had been served by larger (demolished) Roath depot

Exterior

Long multi-gabled building in red brick; slate roofs with glass lights, stone copings and kneelers. Eleven gables with louvred oculi. Two bays to each gable generally have 2 arched recesses with lunettes. Second and fifth gables have tall doors; large door between gables 9 and 10. To L, early C20 black links with two-storey 4 window block in red brick. Short screen wall to road.

Interior

Steel truss roof structure.

Reason for designation

Listed as rare surviving large building from a major tramway system, illustrating the transport history of Wales' largest city.

- 4.33 The settings of these buildings, with the exception of the Antique Market, relate principally to the railway station in terms of their historic function, and its continued use and presence is key to their significance. All of the buildings are experienced in close proximity to twentieth and twenty-first century development which, particularly in the case of the Antique Market, is dominant within the surroundings. The Station and Water Tower are additionally experienced in the context of the Brains Brewery, with the extant chimney a striking historical feature within the townscape. Views from the south entrance to Cardiff Central, which is of twentieth century construction, looking southward towards the study site, views are characterised by twentieth and twenty-first century buildings, with the Clayton Hotel, St Patrick's House and the tower element of the BBC Wales building being dominant within the skyline. The study site, in its present condition is not experienced within the setting of this group of heritage assets.

Impact of Development Proposal

- 4.34 Whilst the 30 storey proposed towers constitute a change within the built environment of the above assets, none of the listed group has landmark character contributing to its reasons for designation and significance. The settings of all of the assets are heavily characterised by recent development, including tall structures, and in this context the proposals are not considered to make a significant material change, in terms of impacting on the assets' sensitivity. TVIA montage No 6 produced for the Townscape and Visual Assessment by Axis Mason (Plate 21) evidences that the Proposed Scheme sits well within the contextual development of its surrounding in this view. Those assets whose settings relate to the railway will continue to maintain this connection, and the Antiques Market, on account of the enclosed nature of its environs is already severed from the wider context. On account of these factors, the tower elements of the proposals are not considered to harm the significance of any of these designated heritage assets.

Statue of the Second Marquess of Bute

Description and setting

- 4.35 This Grade II listed statue (13799) is located c. 460m to the north-east of the study site. The listing description is as follows:

History

1849-53; by John Evan Thomas. Originally stood outside the old Guildhall at N end of St Mary Street before being moved to the S end of the street. The Second Marquess laid the foundations of modern Cardiff by the development of the Bute East and West Docks.

Exterior

Standing bronze figure on granite base.

Reason for designation

Early example of Victorian public sculpture commemorating the founder of modern Cardiff.

- 4.36 The setting of the asset now relates primarily to Callaghan Square to which the statue has been located facing northward toward the city centre. The landscaped character of the park generates an appropriate setting for the asset, having a degree of aesthetic value and allowing the prominent experience of the statue. The asset is predominantly experienced in the context of twentieth and twenty-first century buildings, but the towers of the Church of St Mary are glimpsed in southward

views, generating a very small degree of historic context. In its present state the study site is not experienced in the setting of the statue.

Impact of Development Proposal

- 4.37 The thirty storey towers will be experienced within the wider setting of the asset, and will be a notable feature on the southern skyline, competing with the landmark character of the Church towers. This character is, however, very much reduced by the very partial and fragmentary nature of the glimpsed views as demonstrated by the TVIA Montage No 8, prepared by Axis Mason for the EIA (Plate 22), with the scale of the proposed development largely being in character with the later twentieth and twenty-first century setting of the asset. Given that the original location of the statue did not allow for the co-visibility with the Church of St Mary originally, it is considered that this visual relationship is of negligible value, and as the proposed towers do not occlude the towers there is no detrimental impact to this aspect of the statue's setting. The significance of the statue is therefore considered to be preserved.

Docks Warehousing and Bridges building group

- 4.38 These five designated assets, all listed at Grade II, will be described separately, but treated collectively with regard to their settings.

Descriptions and collective setting

Marquess of Bute's Warehouse

- 4.39 This Grade II listed building (13982) is located c. 580m to the north-east of the study site. It is described as follows in its listing details:

History

On map of 1849. Earliest surviving dock warehouse in Cardiff. Built (and probably designed) by William Catleugh, millwright and iron founder of the Hayes, Cardiff. Later used as a bonded warehouse.

Exterior

Square 3 storey and basement. 3-bay warehouse with coursed hammer-dressed facing. Low pyramidal slate roof behind ashlar blocking course and heavy cornice; small chimney stack. Full height loading bay to centre with boarded and corrugated shutters on three level, brackets for pulley hoists over. Cambered heads to flanking windows with 20-pane ironwork frames (now blocked). Similar N front with boarded doors retains fine ironwork bracket crane with pulley; small enclosed yard.

Interior

Retains original construction with three-aisled ground floor over fireproof basement. Cast-iron, fish bellied and crossed-flanged piers with annulets; cross-plate ends curved. Embossed lettering on piers reads; "W. Catleugh. Millwright, Cardiff". Cast iron handrail to corner stairs down to basement with brick Jack Arches and inverted T-profile girders supported by longitudinal girders with piers as before. Stone flagged floor. Timber upper floors and pyramidal timber roof of king post construction central support. Brick linings to upper floors only.

Reason for designation

Earliest surviving dock warehouse in Cardiff. Group value

Warehouse adjacent to Marquess of Bute's Warehouse

- 4.40 This Grade II listed building (13983) is located c. 575m to the north-east of the study site. It is described as follows in its listing details:

Later C19 warehouse. Hipped slate roof, walls in snecked brown stone, with rounded corners. Three storeys. Five bays to S front, second and fourth bays full height loading bays, small gabled hood over fourth bay. Camber-headed windows with stone sills, largely retaining stone pane wooden glazing. Three bays facing W in similar style, L bay on ground floor obscured by connecting passage to adjacent warehouse to W. North elevation has ground floor obscured by added passage, but to R, 3 bays in same style as S front (i.e. top of loading bay flanked by 2 windows). To L, wing at right angles, 3 storeys, full-height loading bay flanked by camber-headed windows; side elevations blank, in red brick. Interior has to W; 3 bays divided by 2 rows each of 3 cruciform-sectioned iron columns

supporting wooden beams on which wooden floors are laid. To E, beyond wall, single row of 6 cast-iron columns continued into N wing supporting wooden floors in same way.

Reason for designation

Part of a group of 3 well preserved C19 warehouses on former N quayside of Bute West Dock. Group value.

Warehouse on corner of Lloyd George Avenue and Herbert Street

- 4.41 This Grade II listed building (13984) is located c. 565m to the north-east of the study site. It is described as follows in its listing details:

Exterior

Later C19 four storey warehouse. Hipped slate roof, steeply pitched. Sneaked brown stone, on battered plinth. Red brick string courses. Rounded corners except to NE. Parapet in pale cream limestone with shallow cornice and arcading at eaves level does not continue on N side of building and has been rebuilt in brick to S. Four storeys, five bays to S, central bay full-height loading bay. Windows with four-centred arches now largely blocked; red brick string courses at sill level but the longer windows to L on ground and first floor sink below string course. Five bays to W in similar style. To N, 3 windows on second and third floors, 2 on ground and first floors, plus inserted square-headed windows. Low square-headed basement windows. To E, two windows to each floor, closely grouped; extension in stone and red brick connects with adjacent warehouse and partially obscures elevation.

Interior

Cast iron columns I-beams on which wooden floors are laid.

Reason for designation

Part of a group of 3 well preserved C19 warehouses on former N quayside of Bute West Dock. Group value.

Bridge over Dock Feeder Canal (13989)

- 4.42 This Grade II listed building is located c. 630m to the north-east of the study site. It is described as follows in its listing details:

History

Bridge probably dates from 1850's when last phase of Bute East Dock was under construction and formerly carried traffic over Junction Canal which connected Bute West and East Docks. Now footbridge in Atlantic Wharf development.

Exterior

Heavy rock-faced abutments with parapet in slightly smaller blocks. Central span of bridge set back from abutments. Smoothed-dressed band course at decking level. Rounded rock-like coping stones to span of bridge but flat coping stones to flared approaches. Canal is spanned by 4 parallel I-beams which are bound together by transverse tie rods. Flanges on I-beams support shallow brick arches on which modern brick decking is bedded.

Reason for designation

Listed as one of a pair of small bridges surviving from early period of Bute E Dock. Group value with adjacent bridge.

Bridge over Dock Feeder Canal (13988)

- 4.43 This Grade II listed building is located c. 740m to the north-east of the study site. It is described as follows in its listing details:

History

Bridge probably dates from 1850's when last phase of Bute East Dock was under construction and formerly carried traffic over Junction Canal which connected Bute West and East Docks. Now footbridge in Atlantic Wharf development.

Exterior

Bridge has heavy abutments in rock-faced coursed brown stone with parapet in slightly smaller blocks. Smooth dressed blocking course at decking level. Parapet returns at corners of bridge to form refuges which are probably built from parapet wall to former approaches. Span of bridge slightly set back from abutments; buttresses on E and W of abutment on S side. Rounded rock-like coping stones

to span of bridge and refuges, but flat coping stones to approaches. Bridge now approached by steps (formerly ramped) and has modern brick decking. Canal is spanned by six parallel I-beams which are bound together by transverse tie rods. Flanges on I-beams support shallow brick arches on which decking is bedded. Beneath bridge on N side on canal is later stone infill blocking towpath and supporting bridge.

Reason for designation

Listed as one of a pair of small bridges surviving from early period of Bute E Dock. Group value with adjacent bridge.

Spillers and Bakers

- 4.44 This Grade II listed building (14016) is located c. 820m to the north-east of the study site. It is described as follows in its listing details:

History

Dated 1893 in parapet. Architects probably Veall & Sant. Formerly part of Spiller's mills and factory with curved elevation to NW following the line of former railway. Converted to flats late 1980's by AWT Architects.

Exterior

North West elevation in snecked small blocks of brown stone with dressings and vertical and horizontal articulation in red brick, red brick plinth, cornice and some details in pale limestone. Modern small-pane glazing. Generally of 5 storeys above double-height ground floor, plus penthouse storey. Triangular plan with curved side to NW, and rounded corners to this elevation. Asymmetrical roof with modern covering and windows to penthouse flats. Red brick parapet with, in asymmetrically-placed central stepped section, inscription 'Spillers and Bakers Ltd' and date '1893'; above inscription, shaped gable with carved relief of sheaf of corn. Fifteen window bays to NW, first and tenth from L formerly full-height loading bays now glazed and balconied. Attic storey with camber headed windows, 4 beneath inscription with keystones. Below this, dentil cornice in limestone. Below cornice, 4 storeys with windows following attic storey, but without keystones. Windows in next storey down set in deeper surrounds. Ground floor generally double-height with tall round-headed arches (bays 6 and 7 from L now entrance with modern canopy), but bays 2 and 11 from L have 2 storeys with round window above camber-headed window. Elevation to S in simpler version of same style, in rubble with less use of red brick articulation and more varied placing of openings (which still have red brick dressings). Up to 16 window bays (on top floor) with bays 5 and 13 from L full-height loading bays. Elevation to E has rounded gable with limestone coping; rubble, with red brick dressings and quoins to SE corner. Seven storeys plus 2 in gable. Eight bays of which fourth and seventh from L were tall loading bays. In gable apex, triple window with camber-headed openings. Below these, head of former loading bay flanked by triple and quadruple windows with camber-headed windows, paired to L and R of left loading bay, single elsewhere. On first floor, end windows circular.

Interior

Converted to flats in late 1980's.

Reason for designation

Listed as a striking example of industrial architecture with little loss of character in conversion.

Bonded Warehouse

- 4.45 This Grade II listed building (13957) is located c. 950m to the north-east of the study site. It is described as follows in its listing details:

History

Built 1861 as warehouse, to designs of resident dock engineer W S Clarke. Conversion to offices with extension to N, late 1980's.

Exterior

Four storeys, 6 bays wide by 3 deep, built on an iron framework and faced with orange-red brick (English Bond); some dressings in bathstone, grey stone plinth to sides and rear. Hipped roof with modern corrugated covering. Parapet with bathstone capping. Facing Bute East Dock, 6 bays, second and fifth tall loading bays with hoists above; cast iron sills to former doors now with office glazing and wrought-iron balconies. Other bays have small round-headed windows with bathstone keystones and sills; modern single pane glazing. Four cast iron tie-rod ends to each floor. Ground floor facing dock open to almost half depth of building to provide covered working area; modern office glazing at

rear. Overhang supported by broad Tuscan-style cast-iron columns carrying cast-iron beam upon which masonry rests. East and west elevations similar to south with, above ground floor, 3 bays with central loading bay flanked by round-headed windows; cast iron tie-rod ends. Ground floor open to S but brick to N. North elevation has to R, 2 windows on each upper floor. Modern extension in red brick and glass to L (from conversion to offices) covers loading bay and other windows.

Interior

Converted to offices but retains original construction with cast-iron Tuscan columns supporting parallel iron beams spanned by shallow brick arches.

Reason for designation

Listed as fine warehouse building, with interesting iron and brick construction, from early period of Cardiff Docks.

- 4.46 The settings of these designated heritage assets relate particularly to the docks construction of the nineteenth century and gain high levels of illustrative historic value from their interrelation and historic functional association. The buildings are experienced in northward views in the context of twentieth century development to the core of the city, which provides a contrasting backdrop, but does not interfere with the visual interrelationships of the assets themselves, where these exist, which remain legible (Plate 23).

Impact of Development Proposal

- 4.47 The proposed towers will be evident through partial glimpsed views to the south of the assets, but the distance and orientation of the proposed development is such that its presence will not be intrusive within the developed context of the environs. The Warehouses and Spillers and Breakers are structures of scale, but their significance relates to their historic function and dates of construction rather than their contribution to townscape or landmark value. Given the above, it is considered that the proposed towers would not detrimentally impact on the assets' settings, and their significance is preserved.

Railway Bridges group

Railway Overbridge (13962)

- 4.48 This Grade II listed building is located c. 500m to the north-east of the study site. It is described as follows in its listing details:

History

Carried Taff Vale Railway over the former spur of the Glamorganshire Canal leading into Bute West Dock. Canal filled in and arches blocked but railway bridge and embankment remains in use to Cardiff Bay Railway station. Probably constructed 1839-40 when the Bute West dock and Taff Vale Railway to Dock Station were constructed and opened in successive years. George Bush, resident engineer to I K Brunel, the eminent Victorian engineer, laid out the Taff Vale railway and supervised the construction of this bridge.

Exterior

Triple portal masonry bridge with wharf side paths flanking wide canal opening. Central segmental arch with bullnosed voussoirs (stepped on upper side) and coursed facings, plain cappings. Outer bays forward slightly with semi-circular arch rings. Convex and battered revetment wall sweeps forward at S side to meet outer pier set into embankment. Ground level risen considerably at arches but left hand opening returns C19 railings and tapered polygonal ironwork piers (stamped Baylis and Co) to wide pedestrian gate with punched Gothic detailing to hinge plate. Part remains of railings to right hand arch. The E side facing Collingdon Road appears similar but is partly masked by later widening of railway with bow-shaped plated girder bridge. Heavily overgrown at time of inspection (autumn 1997).

Reason for designation

An exceptionally early railway overbridge with fine masonry, on historically important Taff Vale Railway.

Railway Overbridge (13987)

- 4.49 This Grade II listed building is located c. 670m to the north-east of the study site. It is described as follows in its listing details:

History

Built to carry the Taff Vale Railway over the feeder to the West Bute Dock. Possibly constructed 1839-40 when West Bute Dock and Taff Vale Railway were constructed; George Bush, resident engineer to I K Brunel laid out railway and supervised construction. Alternatively may have replaced a temporary timber bridge in 1850s or 60s. The bridge has been widened - see changes to brickwork on underside.

Exterior

Built on a skew. Coursed rock-faced rubble masonry with dressed coping and stepped stringcourse over arch. Long thin voussoirs to broad elliptical arch. Ramped and tapered pilaster buttresses and ramped abutments to each side, that to SW side has sweeping curve ending in low pier with coping. Similar masonry treatment at N end. The underside has yellow brick at S end and red brick to N. Towpaths to both banks of the feeder, closed at S end by iron gates with finials.

Reason for designation

Included as an exceptional early skew railway overbridge with fine architectural quality and for its special interest to the history of the Taff Vale Railway.

- 4.50 The settings of these designated heritage assets relate principally to the railway line that they span, but in the case of the bridge in closer proximity to the study site, there is also a visual (and historic functional) relationship with the historic warehousing off Tyndall Road. A Glimpsed view of the Church of St Mary is also available, although obscured by foliage and tree planting, which provides a further element of historic context.

Impact of Development Proposal

- 4.51 As described in relation to the Church of St Mary, the presence of the proposed towers within the environs generates a degree of competition to the landmark status of this building, with the consequent diminution of its illustrative value. However, in the context of the overbridges, this has little bearing on their reason for designation and significance, and their relationship to each other, the railway and associated warehousing structures will remain entirely legible. It is therefore considered that whilst the proposed towers will create a change within the environment, the significance of the assets derived from their settings will remain intact and they are preserved from harm.

Conservation Areas

St Mary's Street Conservation Area

Description and setting

- 4.52 The St Marys Street Conservation Area is a roughly triangular area which, although predominantly Victorian in architectural form, retains the medieval street pattern focussed around the St John the Baptist Church and Cardiff Castle. The narrow and irregular character of the historic thoroughfares and the residual traces of burgage plots, evident through the narrow proportions of street frontages, are evidence of the origins of the town.
- 4.53 The north/south axial route is considered to be a key feature of the Conservation Area's character, with the changing course of the Taff between 1600 and 1800 meaning that the southern half of St Mary Street saw a substantial change in layout during this time.
- 4.54 The arrival of the canal and the railway during the first half of the nineteenth century lead to many new opportunities in terms of commercial ventures, and the architecture of this area reflects the commercial aspirations and intentions with the erection of large commercial schemes such as Market Buildings, Howells Department store and the Royal Hotel.
- 4.55 The area's special architectural interest is derived from The use of Portland stone, the classical styling of monumental columns and pediments and gothic ornamentation, which all give the area an opulent style and demonstrate a significant level of craftsmanship particularly in stonework. The variety and juxtaposition of styles and materials, whilst retaining a reasonably consistent building height, is key to the architectural character of the area, as is the presence of a continuous back-of-pavement building frontages. In the streets surrounding St Mary Street, the scale of buildings changes to a more pedestrian character, whilst the warehouse architecture within the area is evidence of the historic quayside location prior to later nineteenth-century redevelopment.

- 4.56 The Conservation Area Appraisal identifies significant views and landmarks within the area, and these are predominantly focused northward towards the Castle, with eastward and westward views also highlighted. Whilst there are landmark frontages identified to the southern end of the Conservation Area, these are experienced principally in views northward, rather than in southerly views.
- 4.57 Whilst the contribution of the setting of the St Mary Street Conservation Area is not specifically addressed as a separate entity within the Appraisal, the key views of the Castle (which lies in the neighbouring Cathays Park Conservation Area) and the Millennium Stadium (lying outside the Conservation Area to the west) indicate that these are the principal identified contributors of setting to the character and appearance of the Conservation Area. As discussed above, however, there are a significant number of Grade II listed buildings lying to the south of the Conservation Area which are illustrative of the nineteenth-century development of the railway and docks, which consequently generate a degree of historic value to the setting of the Conservation Area. The study site is presently not experienced in southward views out of the Conservation Area, with the residential tower blocks at Overstone Court to the south of the study site being the visible landmarks.

Impact of Development Proposal

- 4.58 The presence of the two proposed towers will alter the skyline to the south of the Conservation Area, but will not be experienced in relation to key views associated with its heritage assets. The railway bridge severs long views southward, and the southern setting of the Conservation area is characterised by tall hotel buildings, such as the Carlton and Radisson Blu, the scale of which – thought less than the proposed – is magnified by proximity. The towers of the Church of St Mary are viewed from St Mary Street, and the proposed towers will be experienced in this context, but the severance generated by the railway bridge prevents any experience of this juxtaposition from within the conservation area (Plate 24). Taking the above into consideration, the proposals are not considered to impact negatively on the character and appearance of the St Mary Street Conservation Area, and it is preserved from harm.

Mount Stuart Square Conservation Area

Description and setting

- 4.59 Designated in July 1980 in recognition of the historic special interest evidenced through the commercial growth of Cardiff in the nineteenth century, the Mount Stuart Square Conservation Area displays *an eclectic mix of buildings of different scale style that visually tells a story of how the area developed, prospered, declined and then started to adapt to a new role in the late twentieth and early twenty first centuries* (Cardiff Council, 2009).
- 4.60 The character of the conservation area is described as commercial and maritime, with the remaining historic fabric detailing the residential origins of the area and its progression into a commercial centre in the later nineteenth century. The high concentration of listed buildings within the Conservation Area represents the most notable architects of the period, with the grandeur and elaborate detailing of the commercial buildings being a vivid reminder of the affluence generated by the Welsh coal trade.
- 4.61 Historic development is densely concentrated and of relatively fine grain, with the continuous back-of-pavement building frontage generating a strong sense of enclosure. The buildings are arranged in a linear form along the two converging streets, with the prominent and acute junctions of the converging thoroughfares generating a nodal point which is articulated by striking frontages, particularly that of the National Westminster Bank.
- 4.62 A key characteristic identified is the variance in scale between the domestic two storey buildings and the commercial buildings, some of which are of five-storey height. This characteristic can leave the side and rear elevations of the larger buildings inadvertently exposed, with the rear elevation of the Grade II* listed Empire House cited as a particularly striking example.
- 4.63 Views and vistas within the Conservation Area are principally focussed along the thoroughfares, as the height of commercial development creates viewing channels, which in some cases occlude views of other historic buildings, such as the Cardiff Coal Exchange. Important views into the Conservation Area are those identified from within the adjacent Pierhead Conservation Area, looking north-west, and a south-westerly view into the Conservation Area from the north, in the vicinity of the Grade II* listed Cardiff Bay Station. The return vista toward the Pierhead Conservation Area is highlighted as

the principal outward view, although there is a northern view from James Street, associated with the ribbon of green infrastructure along the western boundary of the Conservation Area.

- 4.64 The setting of the Conservation Area comprises Butetown to the north, largely screened in identified views northward by tree planting, with the view terminated by the Radisson Blu hotel, and Pierhead to the south-east, where the Victorian Pierhead building is strikingly framed by the regenerated landscaping of the quayside development and the Wales Millennium Centre. In the return views from in the vicinity of the Pierhead building, the Conservation Area is experienced through the frontages of the buildings on Lloyd George Avenue. In its present state the study site is not experienced in the context of the Mount Stuart Square Conservation Area. The greatest contribution to significance made by the setting of the asset is the visual relationship with the Pierhead Conservation Area adjacent (Plate 25), which generates illustrative, architectural and aesthetic value.

Impact of Development Proposal

- 4.65 The proposals will be experienced in the wider environs of the Mount Stuart Square Conservation Area in views associated with the Grade II* listed Cardiff Bay Station. However, the distance and orientation of the proposals is such that their scale is not considered to be detracting to the point of harming the significance of the Conservation Area, as the views of the Station in its historic context (looking south) are not impacted by the proposals, which are only experienced in north-westerly views and largely relate to the development within Butetown and Cardiff City Centre. It is therefore considered that the significance of the Mount Stuart Square Conservation Area, which is also derived from Empire House, many other buildings within the environs, and views out towards Pierhead, remains preserved by the proposals, and is not harmed.

Pierhead Conservation Area

Description and setting

- 4.66 Designated in 1984 and extended in 1992, the Pierhead Conservation Area is concentrated on the docks area and associated workers housing, which originated during the nineteenth-century expansion of Cardiff due to the increasing prosperity and trade brought about by coal production in the region.
- 4.67 The residential terraces of Windsor Terrace, Windsor Esplanade and Bute Crescent are of 2.5 storeys and 3 – 4 storeys height (in the case of the latter). Windsor terrace and Esplanade are of brick with white quoined detailing, with the rhythm of the facades punctuated by ground floor bay windows, while the attic dormers have Bute Crescent is more classical in its appearance, stuccoed, with diminishing proportions, cornices and pedimented window detailing.
- 4.68 The three Graving Docks, with their monumental structure and functional components generate a visually striking appearance, and are historically important being key operational structures on the former Mount Stuart Shipyard.
- 4.69 The two parts of the Conservation Area contrast and complement each other in terms of scale and function, and together create a more coherent illustration of the historic function and appearance of the area.
- 4.70 Key Views and focal points relate to views northward into the Mount Stuart Conservation Area, the historic and modern buildings within the Pierhead Conservation Area, and views across the bay in numerous directions, with the Pierhead Building being a key focal point. The open and expansive views southward are particularly key to the character of the area as they allow for the experience of the dockside character, which is unique to this Conservation Area.
- 4.71 The Conservation Area is also characterised by the manner in which twentieth and twenty-first century development compliments and enhances the remaining historic buildings. The open nature of the area assists in this, but the careful use of scale, form, material type and massing in the later works ensures that the heritage value of the designated assets remains entirely legible and constitutes an enhancement to the setting of the assets.

- 4.72 The setting of the Pierhead Conservation Area is dominated by its waterside location, and in terms of illustrative and aesthetic value it greatly contributes to the character and appearance of the Conservation Area. The intervisibility of Pierhead Conservation Area with the adjacent Mount Stuart Square Conservation Area is also a key element of the area's setting, providing important historic context, and some architectural and aesthetic value – although the experience of these are somewhat lessened by distance. In its current state, whilst it falls within the vista out from the Pierhead Building, the study site is not experienced owing to the low-scale nature of its development.

Impact of Development Proposal

The towers will be experienced in the context of the Pierhead Conservation Area in views north (Plates 27 and 28), although it has been demonstrated in section 4.18 that the landmark status which contributes to the significance of the Grade I Pierhead building is preserved. Views outward to the designated dockside and focal points in the wider environs, together with the relationship of the Pierhead building to designated assets in its vicinity and the quality of the public realm improvements will not be impacted by the proposals. It is therefore considered that the Pierhead Conservation Area is preserved from harm by the proposals.

Registered Parks and Gardens

Grange Gardens

Description and setting

- 4.73 Designated in 2013, Grange Gardens was constructed between 1891 and 1895 and was the first of Cardiff's public parks to include a bandstand and public tennis courts. The Gardens were restored in 2000 with money from the Heritage Lottery Fund and retain much of their original layout. The Gardens cover an area of c. 1.2ha and have a roughly triangular footprint. There is a perimeter pathway, with five radial pathways which converge centrally at the bandstand. The eastern half of the gardens is now taken up with playing fields, and a newly constructed community centre replacing the previous Grange Pavilion. The Gardens are planted to the perimeter with trees, and two Veteran trees are sited centrally to the park. There are two Grade II listed buildings located within the park, these being the War Memorial (19088), dating from the 1920s, and Shelter in Grange Gardens (19089) which is considered likely to be contemporary with the gardens and is a rustic roughly octagonal structure of tree trunks and branches with windows to front and sides of the octagon and a slate roof. Its function would likely have been as an eyecatcher.
- 4.74 The significance of the asset relates primarily to its early date and the illustrative, aesthetic and architectural values of the designated structures within it.
- 4.75 The setting of the Gardens is the contemporaneous urban residential development in its immediate environs, which endows it with a degree of illustrative value and a limited degree of aesthetic and architectural value. In its present state the study site is not experienced within the setting of the asset.

Impact of Development Proposal

- 4.76 The proposed towers would be evident in views north-east from Grange Gardens, although these views would be somewhat filtered through the tree-planting to the north-eastern boundary as demonstrated by TVIA view No 2 (Plate 26), and would not detract from the tight urban enclosure of the historic residential properties in the Gardens' immediate vicinity, which is experienced in many views outward, and which contributes illustrative value through contemporaneous historic context. As a consequence, the setting is considered to retain its contextual value, and the significance of the asset is not considered to be impacted by the proposals.

Potential Impacts on Non-Designated Heritage Assets

Description

- 4.77 As noted in paragraph 3.16 the subject building is not Locally Listed, and the DBA assessment (Orion 2020) identified the structure as being of a low level of heritage significance, relating to its historic usage in the WW2 war effort. The building is described thus in the DBA report:

[The subject building is a] large linear two storey red-brick building which is the only surviving industrial unit associated with the Curran Engineering Works. The structure is in a disused and poor condition and the interior was largely not accessible. While some of the original features survive, particularly on the eastern and southern elevation, the building has undergone significant alteration including the loss of many of the original window along the south elevation, modification of the original roof with a raised sheet roof and the removal of much of the original industrial fittings (it was possible to view the central part of the building from the northern elevation which confirmed that this area was stripped out/derelict inside).

- 4.78 The structure itself is of standard manufacture and of extremely limited architectural or aesthetic value. Any illustrative value with regard to its historic WW2 function has been removed by the loss of fixtures. The low status of the building has been confirmed by the LPA's previous decision to allow its demolition in relation to approved development of the site.
- 4.79 The only extant element of the setting of the building which relates to its previous industrial function is the remaining sections of track preserved within the road surface, which generate a very small level of illustrative value, eroded by their fragmentary condition.

Impact of Development Proposal

- 4.80 The development proposals would occasion the demolition of the existing building and so the harm would be of the highest magnitude.

5.0 Summary and Conclusions

- 5.1 In summary, the proposals are considered to be compliant with regard to National and Local Government policy.
- 5.2 All designated heritage assets have been demonstrated within this assessment as being preserved from harm to their significance.
- 5.3 TAN 24 states at paragraph 1.29: *The local planning authority will need to make its own assessment of the impact within the setting of a historic asset, having considered the responses received from consultees as part of this process. A judgement has to be made by the consenting authority, on a case-by-case basis, over whether a proposed development may be damaging to the setting of the historic asset, or may enhance or have a neutral impact on the setting by the removal of existing inappropriate development or land use.*

Sources

General

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Cartographic

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 1915-16 Ordnance Survey, 1:10,560 Scale Map
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Orion Heritage (2020) Curran Embankment Cardiff: Historic Environment Desk Based Assessment

Plates



Plate 10: View of Cardiff City skyline from Cardiff Castle Keep



Plate 11: View of the Church of St John the Baptist from St Mary's Street



Plate 12: View of the Pierhead Building in context with twenty-first century development in its environs



Plate 13: View of the Mount Stuart Square Conservation Area looking north-west from the Pierhead Building



Plate 14: View of the Pierhead Building looking north-west from the Norwegian Church across Cardiff Bay



Plate 15: View of Cardiff Bay Station looking north-west toward the study site



Plate 16: View of Cardiff Bay Station in the context of the Mount Stuart Conservation Area, looking south



Plate 17: View of the rear of Empire House in the context of twentieth-century development



Plate 18: View of St Mary the Virgin and Stephen the Martyr looking north-west



Plate 19: View Along St Fagan's Street towards the north



Plate 20: View of the Greek Orthodox Church from Bute Street, looking west



Plate 21: View from Cardiff Central looking south (Axis and Mason TVIA image No 6)



Plate 22: View of the Study Site from the Marquess of Bute's statue (Axis and Mason TVIA image No 8)



Plate 23: View of Grade II historic warehouses in the context of twenty-first century development in their vicinity, looking north-west



Plate 24: View southward towards the study site from the St Mary Street Conservation Area



Plate 25: View from Mount Stuart Square Conservation Area south east towards the Pierhead Conservation Area



Plate 26: View from Grange Gardens towards the study site showing glimpsed views of the proposed development (Axis and Mason TVIA image 2)



Plate 27: 60 degree field of vision modelling view of the Pierhead Building from the Norwegian Church (image supplied by Cardiff City Council)



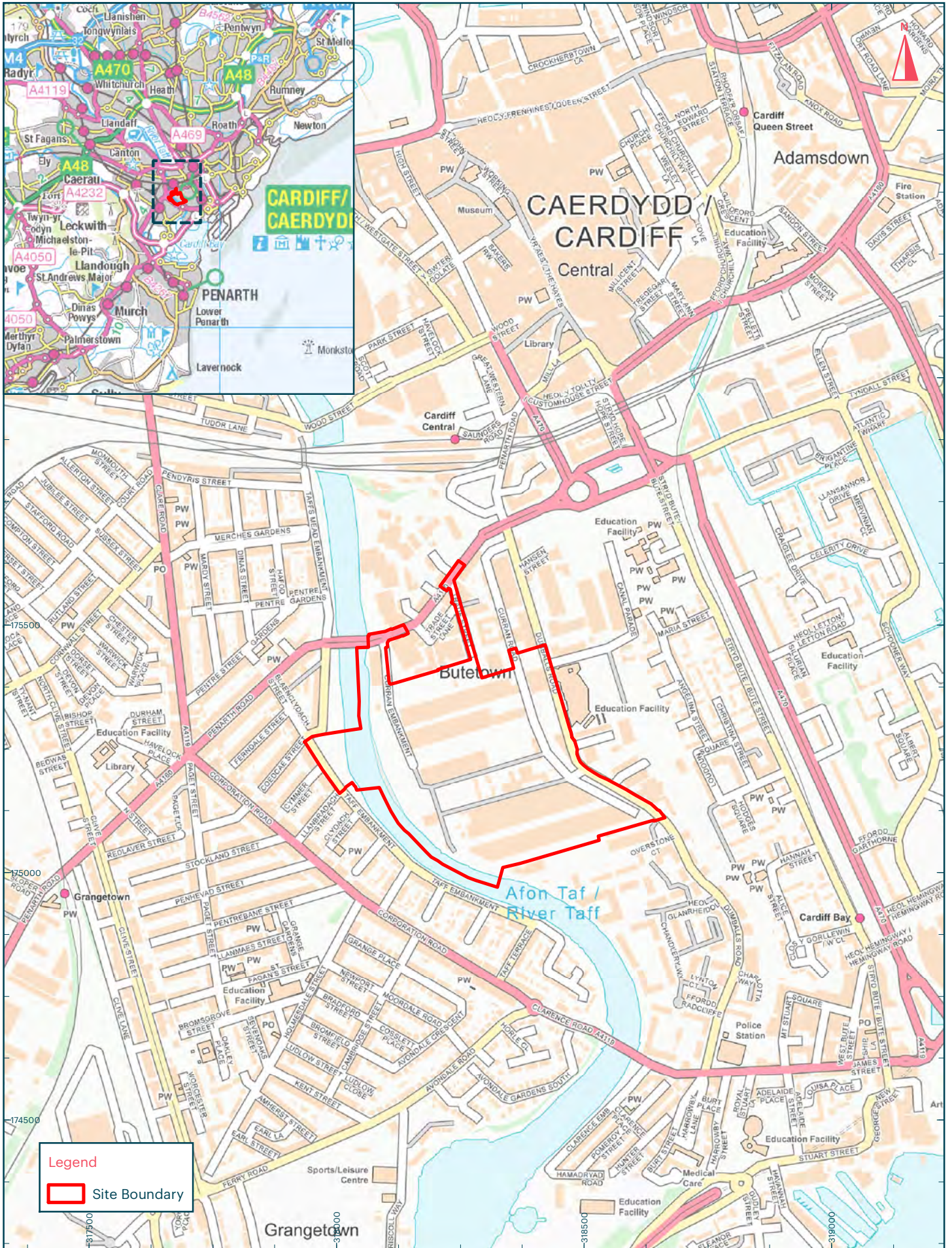
Plate 28: 45 degree field of vision modelling view of the Pierhead Building from the Norwegian Church (image supplied by Cardiff City Council)



Plate 29: 45 degree field of vision modelling view of the Cardiff Bay Station from Hemmingway Road, looking west (image supplied by Cardiff City Council)



Plate 30: 45 degree field of vision modelling view of the Cardiff Bay Station from Hemmingway Road, looking north-west (image supplied by Cardiff City Council)

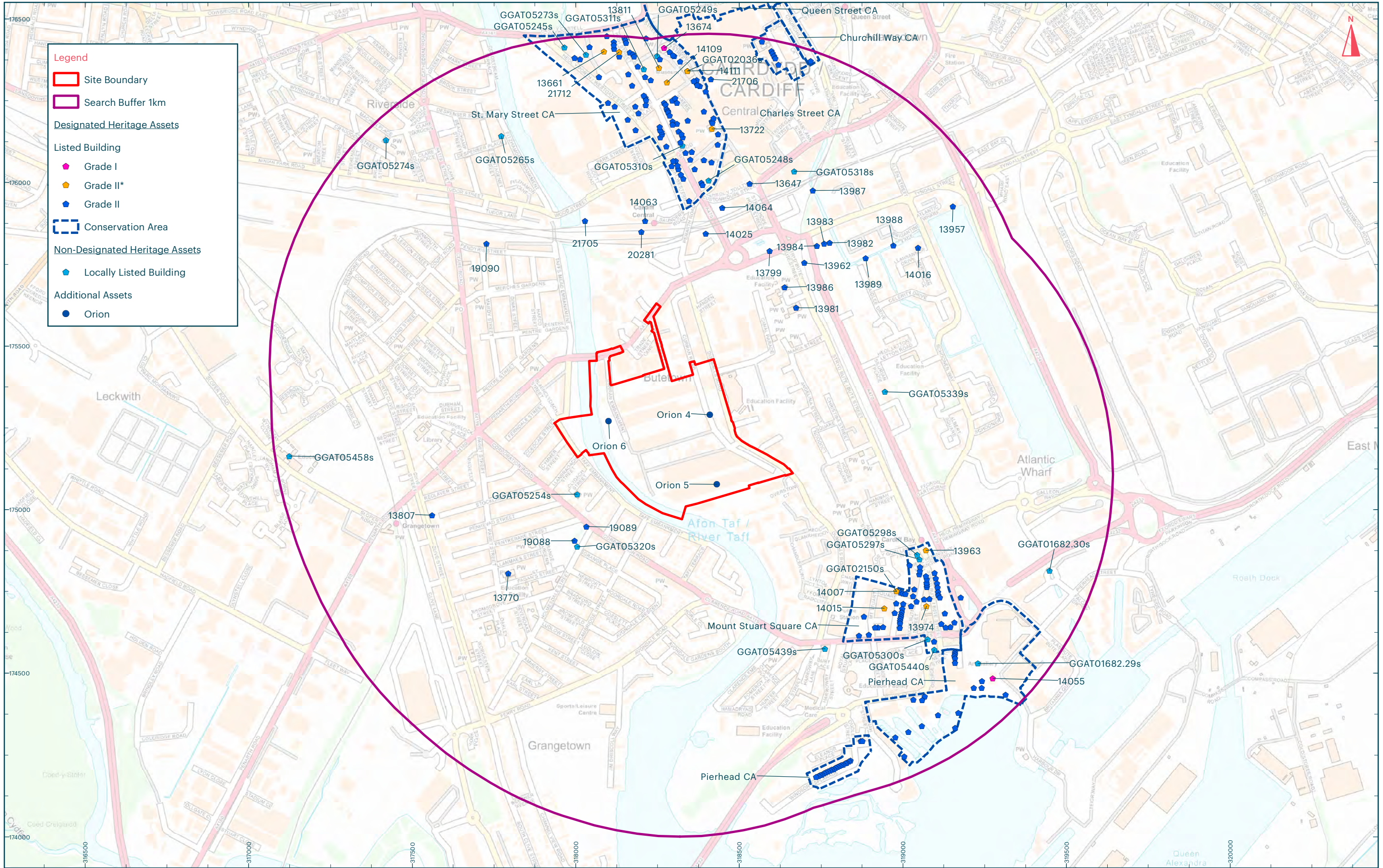


Title:
Figure 1: Site Location
Address:
Curran Road, Cardiff

Scale at A4: 1:10,000
0 300m



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Title:
Figure 2: Location of Built Heritage Assets
Address:
Curran Road, Cardiff

Scale at A3: 1:10,500
0 400m



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Title: Figure 3: 1760 A New and Accurate Map of South Wales by Emmanuel Bowen
Address: Curran Road, Cardiff

Not to Scale:
Illustrative Only





Legend

 Approximate Site Location

Title:
 Figure 4: 1811 Ordnance Survey Drawing
Address:
 Curran Road, Cardiff

Scale at A4: 1:12,000





Title:
Figure 5: 1844 Tithe Survey of St Mary's Parish in Cardiff
Address:
Curran Road, Cardiff

Scale at A4: 1:5,000

0 150m



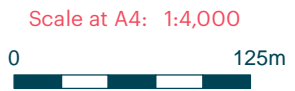


Legend

Site Boundary



Title:
 Figure 6: 1880-82 Ordnance Survey, 1:2,500 Scale Map
Address:
 Curran Road, Cardiff





Title:
 Figure 7: 1879-81 Ordnance Survey, 1:10,560 Scale Map
Address:
 Curran Road, Cardiff

Scale at A4: 1:8,000

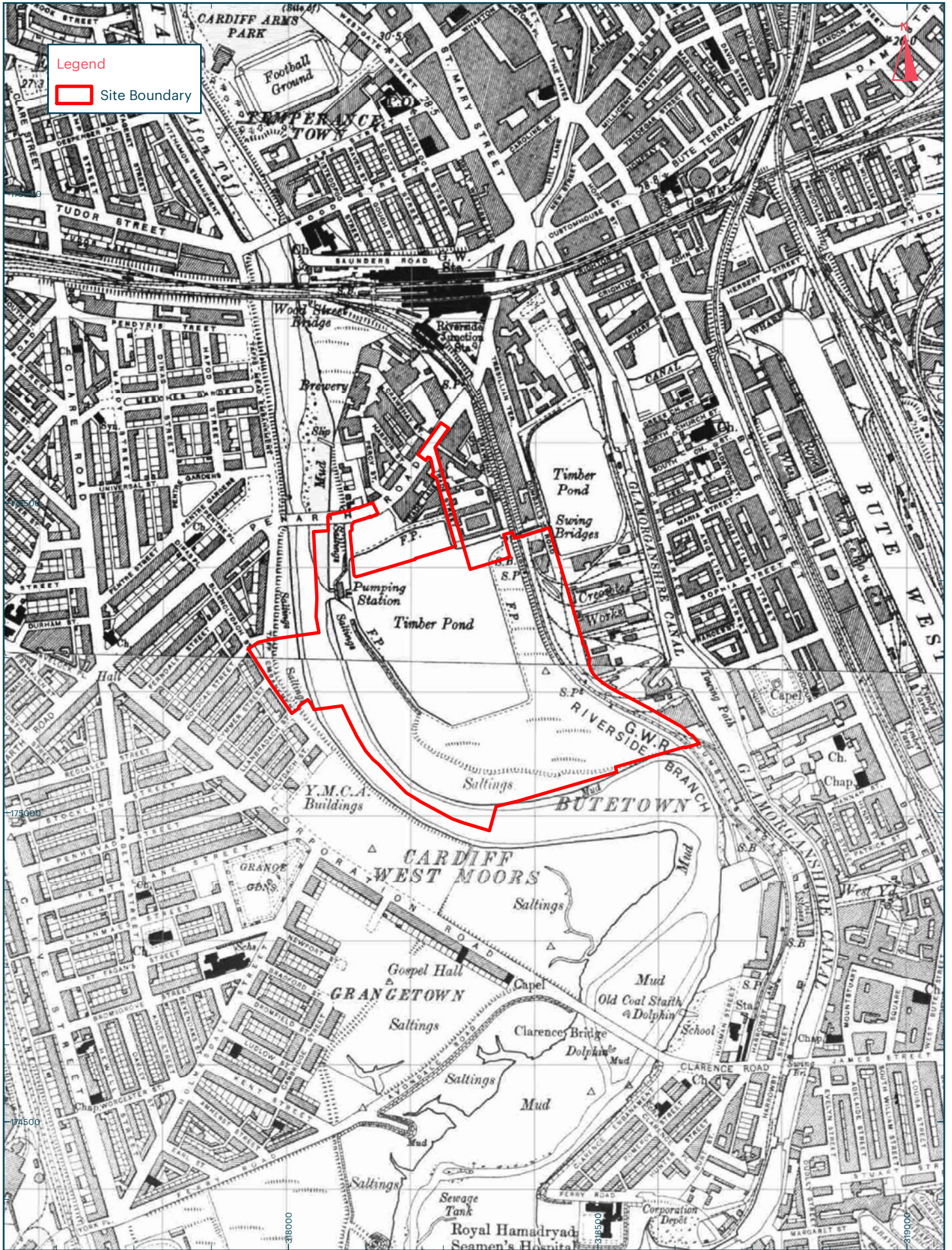




Title:
 Figure 8: 1888 Goad Insurance Plan of Cardiff
Address:
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Scale at A4: 1:4,000





Title:
 Figure 9: 1915-16 Ordnance Survey, 1:10,560 Scale Map
Address:
 Curran Road, Cardiff

Scale at A4: 1:8,000





Title:
Figure 10: 1921 Aerial photograph
Address:
Curran Road, Cardiff

Not to Scale:
Illustrative Only

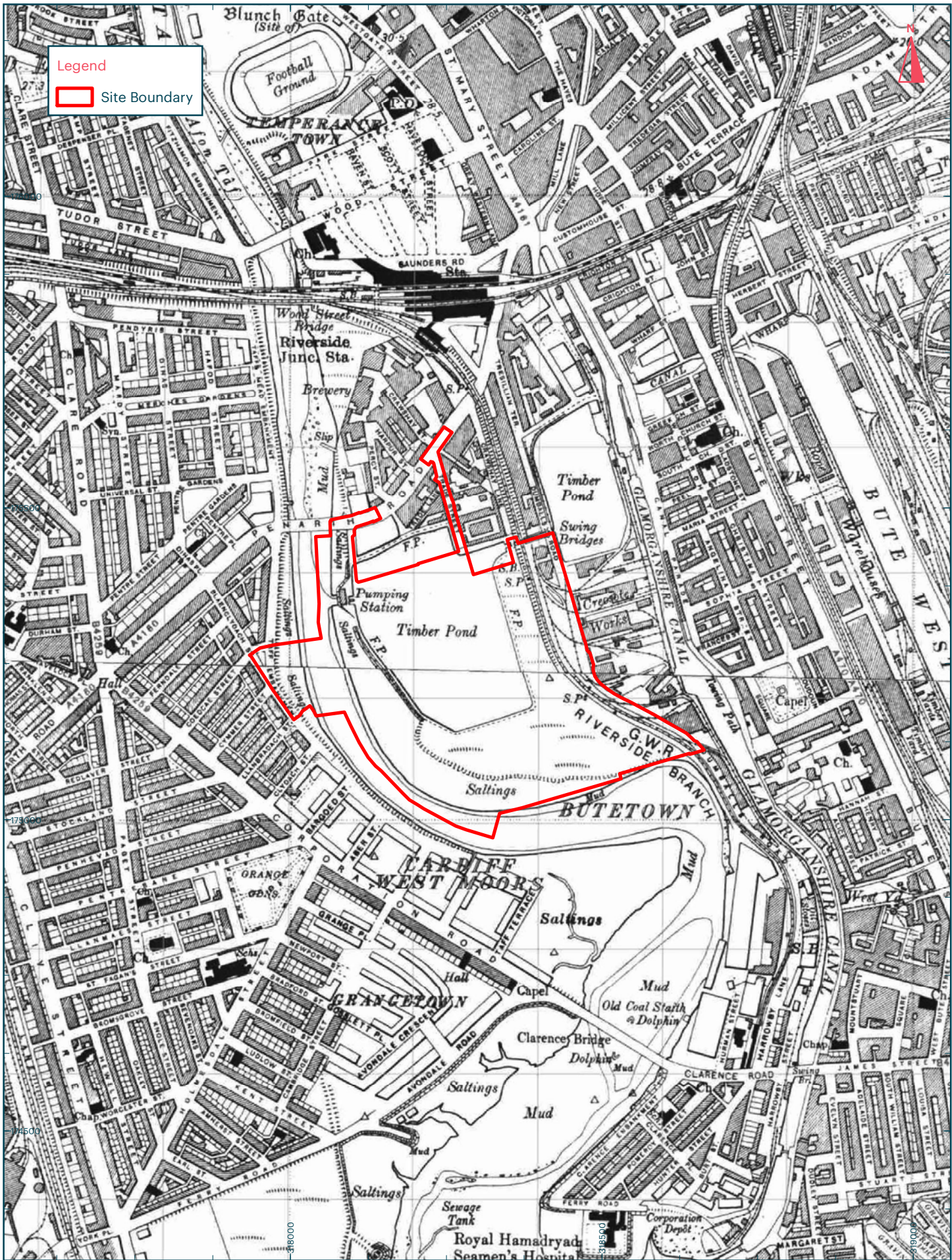




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Figure 11: 1937 Aerial photograph
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Curran Road, Cardiff

Not to Scale:
Illustrative Only

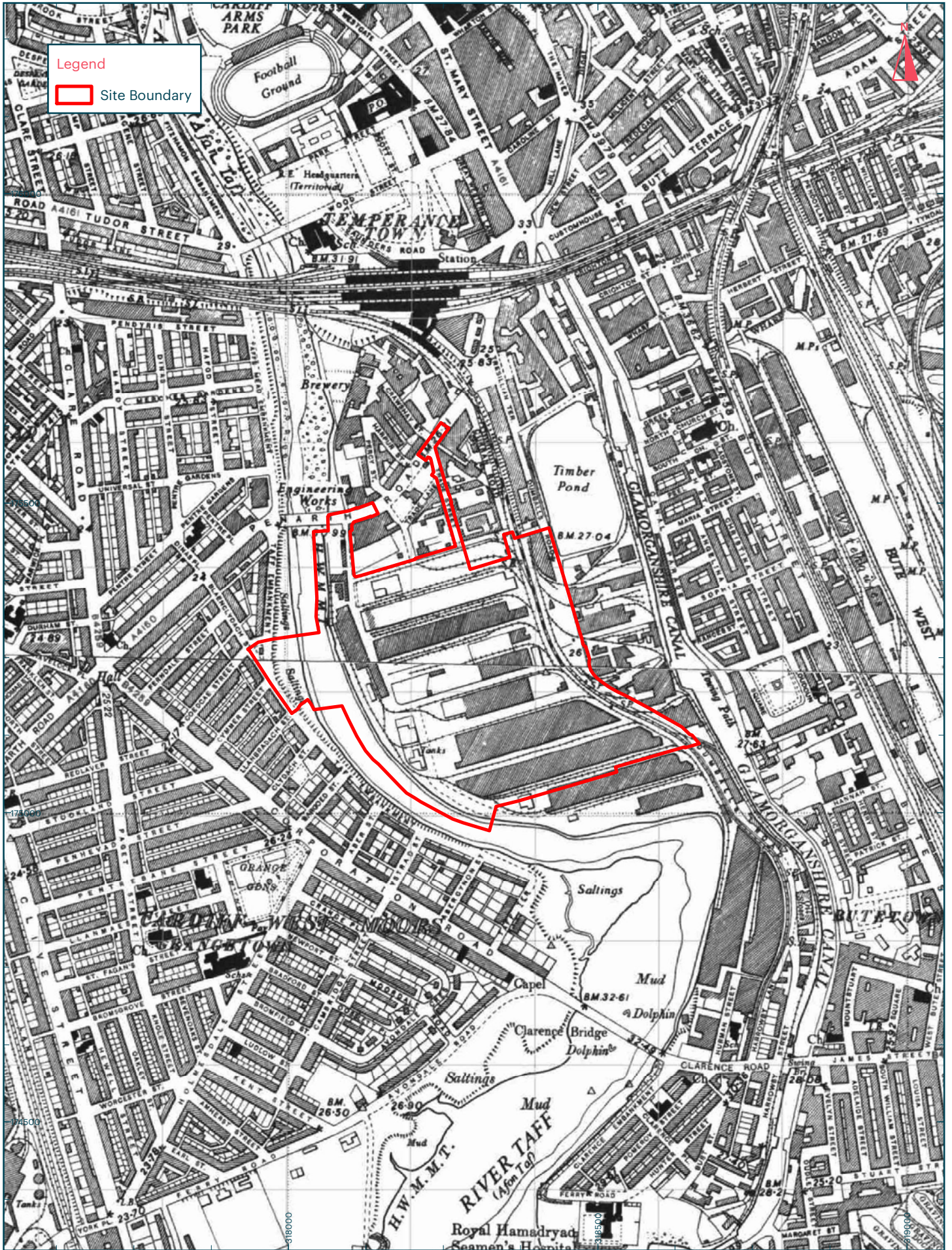




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 Figure 12: 1938 Ordnance Survey, 1:10,560 Scale Map
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Scale at A4: 1:8,000





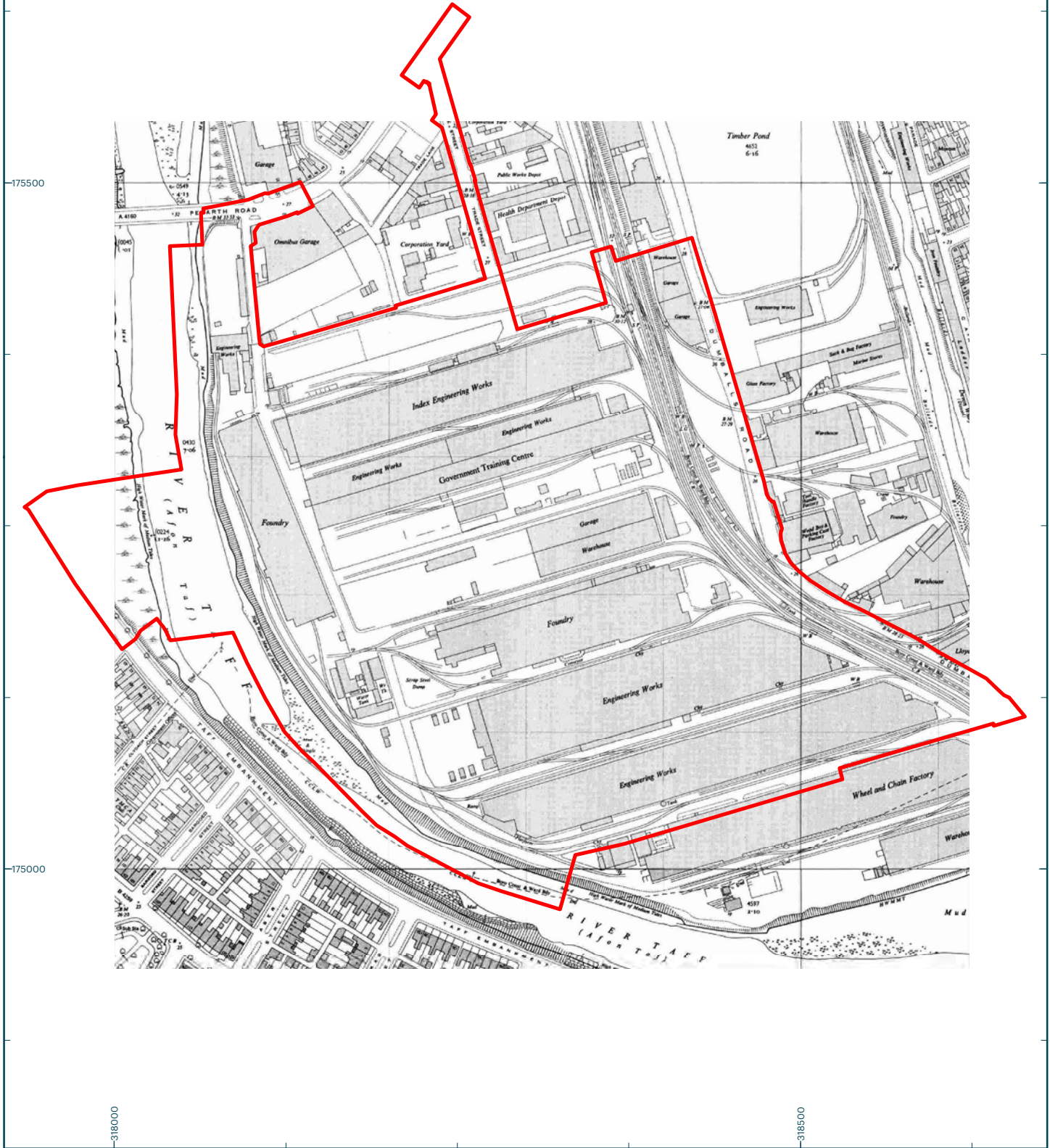
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 Figure 13: 1947 Ordnance Survey, 1:10,560 Scale Map
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 Curran Road, Cardiff





Legend

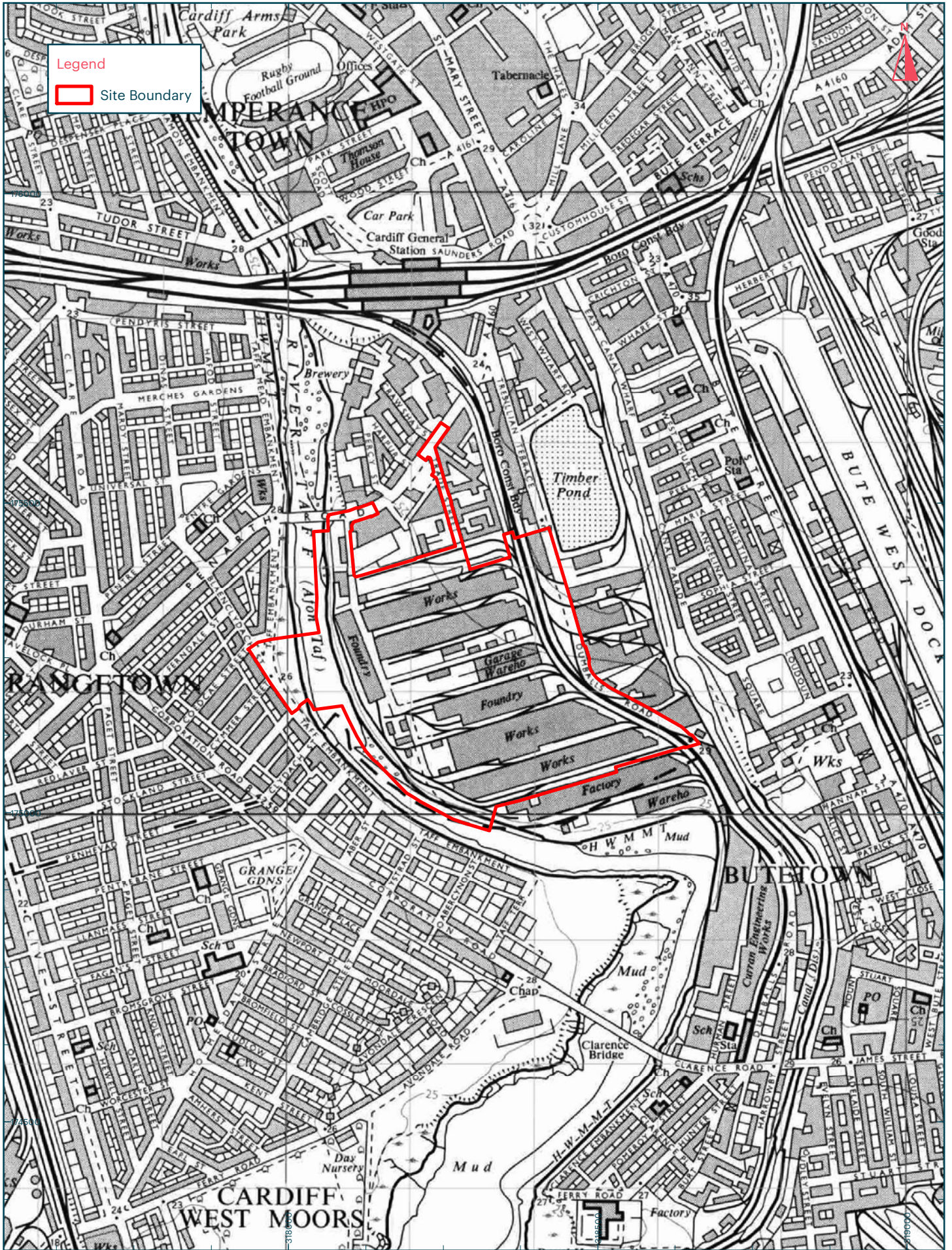
Site Boundary



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 Figure 14: 1951-52 Ordnance Survey, 1:2,500 Scale Map
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Scale at A4: 1:4,000

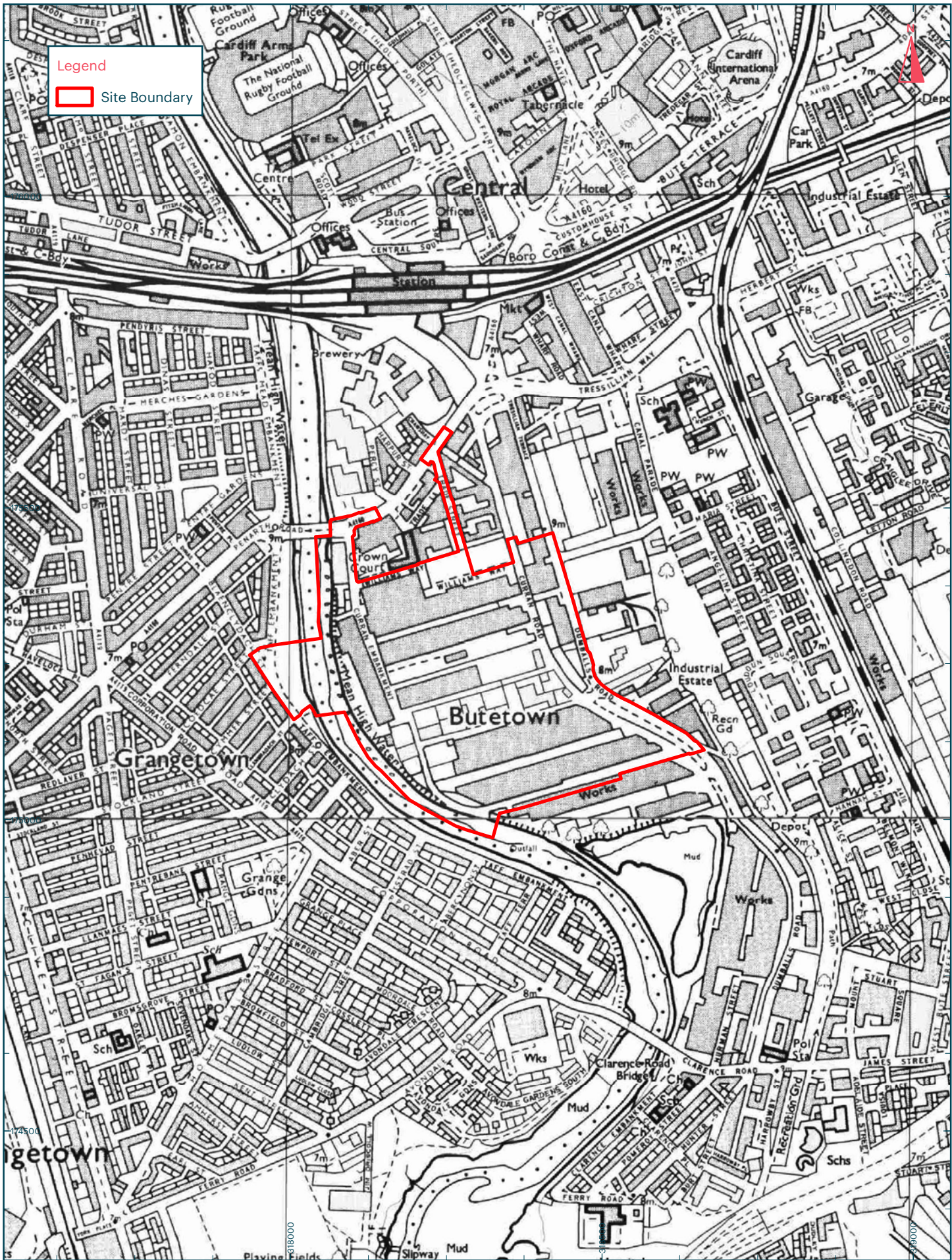




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 Figure 15: 1965 Ordnance Survey, 1:10,560 Scale Map
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Scale at A4: 1:8,000

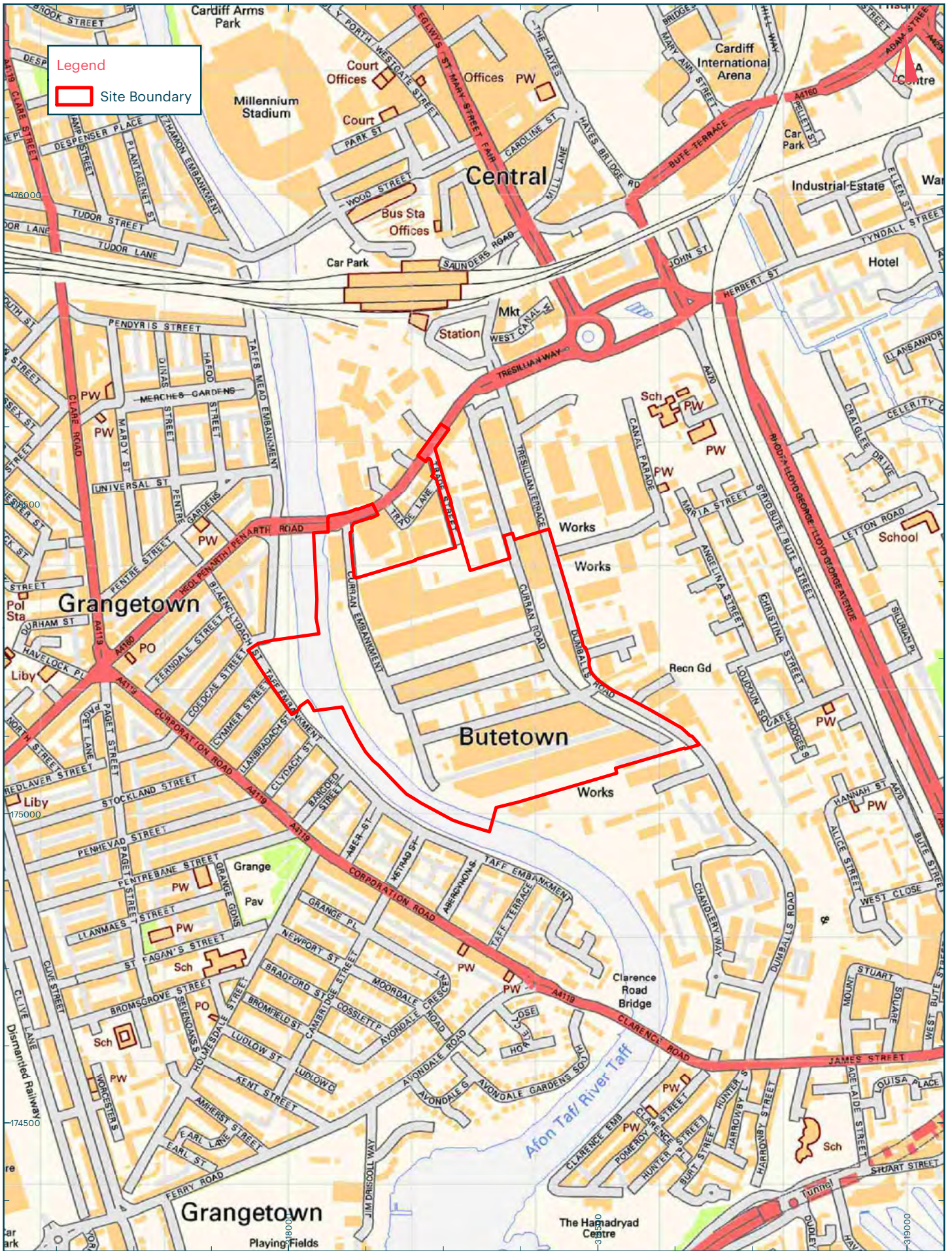




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 Figure 16: 1994-95 Ordnance Survey, 1:10,000 Scale Map
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 Curran Road, Cardiff

Scale at A4: 1:8,000
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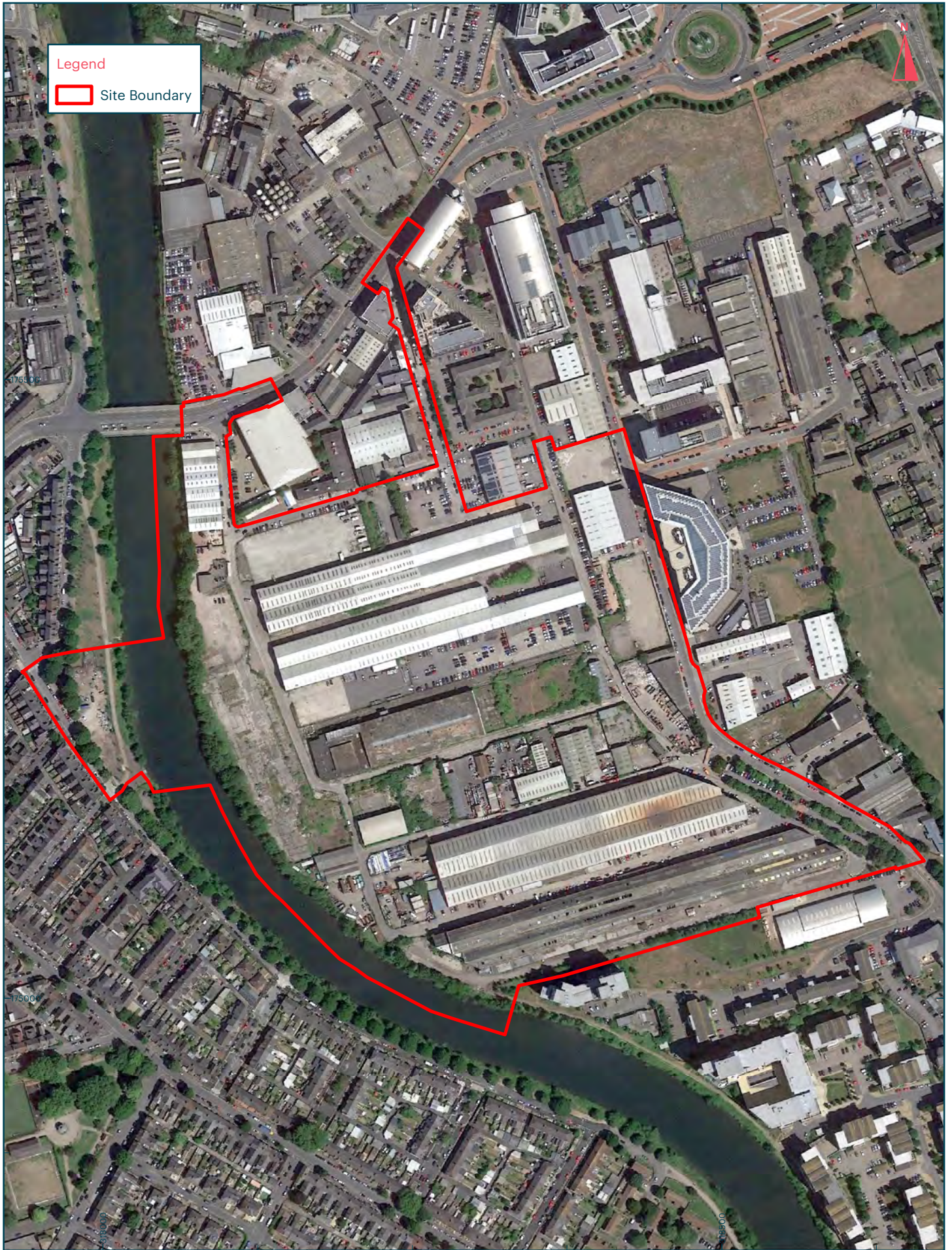




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Figure 17: 2010 Ordnance Survey, 1:10,000 Scale Map
Address:
Curran Road, Cardiff

Scale at A4: 1:8,000





Title:
Figure 18: Aerial View of Study Site (from Google Earth)
Address:
Curran Road, Cardiff

Scale at A4: 1:4,000
0 125m



Appendix 4: Survey Drawings

Our Ref: 7145/NC

26th October 2017

Issued by Email: anilsen@lsh.co.uk

Mr Andrew Nilsen,
Labert Smith Hampton,
3 Callaghan Square,
Cardiff
CF10 5BT.

Dear Mr Nilsen,

RE: Assessment for Proposed Partial Demolition of Industrial Units at Current Road, Cardiff.

With reference to the above site, we undertook a visual inspection of the existing structure in conjunction with LSH on the 18th October 2017. The purpose of the inspection was to determine the form of construction of the existing units, and the implications of a partial demolition of the central units.

Existing Structure

The construction of the units is primarily consistent throughout the development which appears to have been constructed at the same time and is thought to date from circa 1930's to 1940's. The floor plan and typical sections of the building are shown on LSH drawings B1664-01, 02 & 03, and the extent of building under consideration in this report is shown on drawing 7145-SK01.

The primary structure is double bay duo pitch roof, which span across the width of the building. With external columns built in to the external masonry and central columns supporting the internal valley gutter. The trusses are traditional industrial trusses of bolted rolled steel angles (RSA) with a slight camber to the truss bottom cord, and have RSA purlins to support the roof sheeting. The trusses are at 3.8m centres with the internal column spaced at 7.6m centres, with the intermediate trusses supported on valley beams. There are also a corresponding beam to the eaves, which also supported a formed steel gutter. The trusses are connected to the external columns with diagonal plan bracing (RSA's), to both the front and rear elevations. All columns and beam are hot rolled sections, the secondary perimeter columns have rolled steel channels bolted to the flange, protruding out from the wall face.

The building is sub divided into units, the dividing walls are blockwork with integrated steel wind posts, the head of the posts terminate at the underside of truss level and are connected back to internal and central columns with RSA plan bracing. The wall between unit 18 and 9, is of less robust construction, (refer to assessment below).

The external brickwork walls appear to be solid construction however the outer skin is not cross bonded to the inner brickwork. The brickwork is 370mm thick, reducing to 270mm thick above 2m. There are numerous opening in the external elevations, with a combination of concrete lintels over smaller opening, and steel framing to the larger roller shutter openings.

All the units have crane rails, or at least the remaining supporting columns. The crane rail columns correspond to the position of the primary frame columns (at 7.6m centres) and are connected back to the frame columns with bolted steel 'U' bracket cleats, at 4 locations over the column height.

There is a rear annex to a number of the units, which is double height, with a first floor in some areas. The flat roof is a concrete slab supported off lateral steel beams, spaced to coordinate with the main building frame grid. Access was limited to the annex areas.

Unit 18 has the same format as described above, however the front half of the unit between gridlines 31-38 has a higher roof ridge line, with a clad face above the line of the main elevation brickwork. The front elevation columns have been spliced to provide the additional height, and with stub columns off the valley beams. The change in ridge height between the adjacent unit is formed with an add-on truss above the standard roof truss. The crane rail to this unit is also proportionally higher, and has been spliced.

The building is sited immediately east of the river Taff, and the underling ground is fill material to a depth of circa 7m. The foundation to the existing building are unknown.

Condition

The Units are in quite a dilapidated condition with large areas of roof sheeting and roof lights either damaged or missing, as such entry to the units should be undertaken with caution, (especially in severe weather conditions), while wearing appropriate PPE.

There is standing water in some areas with algae and vegetation growth over the slab surface. From a ground level inspection the steelwork appears to generally be in good condition, with some surface corrosion and delamination of the columns, particularly at low level external wall junctions. It is possible that there is further corrosion of elements of the columns which are embedded in the external masonry.

The condition of the slab varies, to provide a clear assessment, all standing water, debris and algae/vegetation would have to be removed.

Large extents of the rear elevation are obscured by vegetation growth, however it is clear that the masonry is in poor condition with diagonal cracking through the brickwork, localised bulges, partial collapse over door heads, and vegetation growth through bed and perp joints. The hidden roof gutters are substantial blocked with debris and vegetation growth, and numerous damaged or missing rain water down pipes, resulting in roof run off discharging down the face of the brickwork.

To provide clear assessment of the external masonry condition the existing vegetation would have to be removed, prior to a detailed inspection, possibly from a MEWP.

Partial Demolition Assessment

The general layout of the existing building is shown on drawing 7145-SK01, with the various primary elements highlighted. Full layout drawing and sections are shown on LSH drawings B1664-01, 02 & 03. It is evident the layout of the units have undergone change over the years, with some sub division and possible removal of original cross walls. Also it is not known when the ridge to Unit 18 was raised, whether it was during construction or some stage later, but the form of construction is consistent with the original, so has been in place for some considerable time.

The lateral stability to the building is likely to be provided by a combination of a number of elements. With the primary columns, roof plan bracing, and eaves walls all contributing to the lateral stability. The current cross walls (gridlines 18 & 24), acting as shear walls in conjunction with the plan bracing are unlikely to be at sufficient centres and capacity to provide effective restraint. This is even more evident at the west end of the building, where walls on gridlines 28 & 31 are partial blockwork with clad light weight steel framing above.

The crane rail support posts are robustly cleated back to the primary columns, which provide the joint benefit to the crane column and frame column, and provide further stability to the building.

Longitudinal stability is provided by the number of column to valley/eaves beams, and the mass of the eaves external brickwork walls proving longitudinal resistance.

The proposed demolition of Units 9 & 10 would require the repositioning of the cross bracing to Unit 10, and repositioning into Unit 11 (gridline 18). The wall, wind post and bracing are currently only providing an internal dividing wall, which is only subject to nominal horizontal loading. If this were to become an external gable elevation an assessment would be required to determine the capacity of the construction to resist wind load, and to support any cladding system that would be used to weather the newly created gable elevation.

Similarly the existing dividing wall/sheeting between Units 18 & 9 (gridline 31), would require replacement as the current clad steel framing above blockwork would not be sufficient to resist wind loading. A plan bracing system would be required to transmit the newly created gable to the existing building. The hybrid truss to the existing change in building height would also require assessment and modification, or more likely replacement.

Partial removal of the masonry elevation would require making good to the wall ends and integrated columns, with suitable strapping tying in provided.

Conclusion

The report summarises the construction of the existing units, and the fundament works required to enable partial demotion proposed, as defined on drawing 7145-SK01.

The existing cross walls are not thought to be contributing to the building stability however a more detailed assessment would be required, as part of the final scheme assessment.

Demolition of the rear annexes would have no effect on the remaining primary structure, except making good to the building envelope, as described for the main units or with masonry.

Forming new gables on gridlines 18 & 31 require new gable structure and adapted bracing to resist the imposed wind loading and to clad the building.

The existing foundations are unknown, however the building is sited in area of known deep made ground, which would have to be considered in the design of any new masonry wall/column foundations. Which are likely to be a pile/ground beam combination.

A full condition survey would be required to determine the condition of the brickwork and slab and steelwork, after clearing of debris and vegetation, via MEWP

Please do not hesitate to contact our office, should you wish to discuss any of the above.

Yours sincerely,



N.J.Clifford

Vale Consultancy

Appendix A

Drawing 7145-SK01

RUMSALLS ROAD UNITS
CARDIFF
EXISTING CONSTRUCTION KEY PLAN

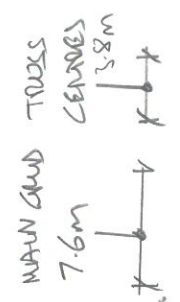
7145 - SK01
 SCALE 1:500

BLOCKWORK WALL WITH
 CLADDING (TWO SIZES) ABOVE
 STEEL FRAME BEHIND
 (NOT SEEN)

CRANE RAIL/COLUMNS

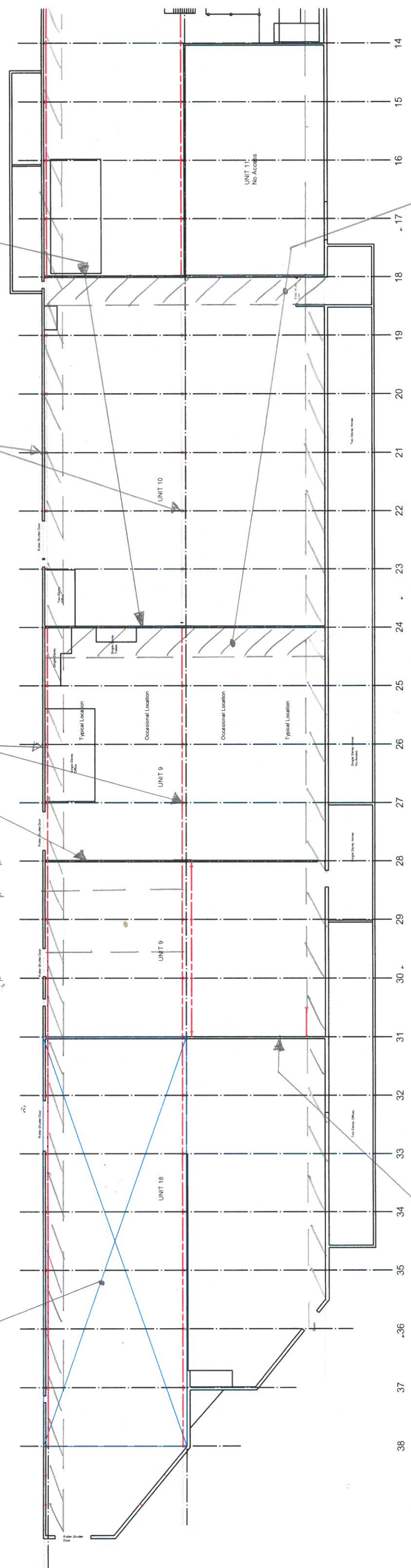
LONGITUDINAL
 SPACING TO
 UNDERSIDE OF
 TRUSSES

ZONE WITH
 HIGHER ROOF
 PURGE LINE



215 THICK BLOCKWORK
 ON CONCRETE PLUMB
 WITH STEEL WIND POSTS
 AT 3.6M CENTRES

CRANE
 COLUMNS



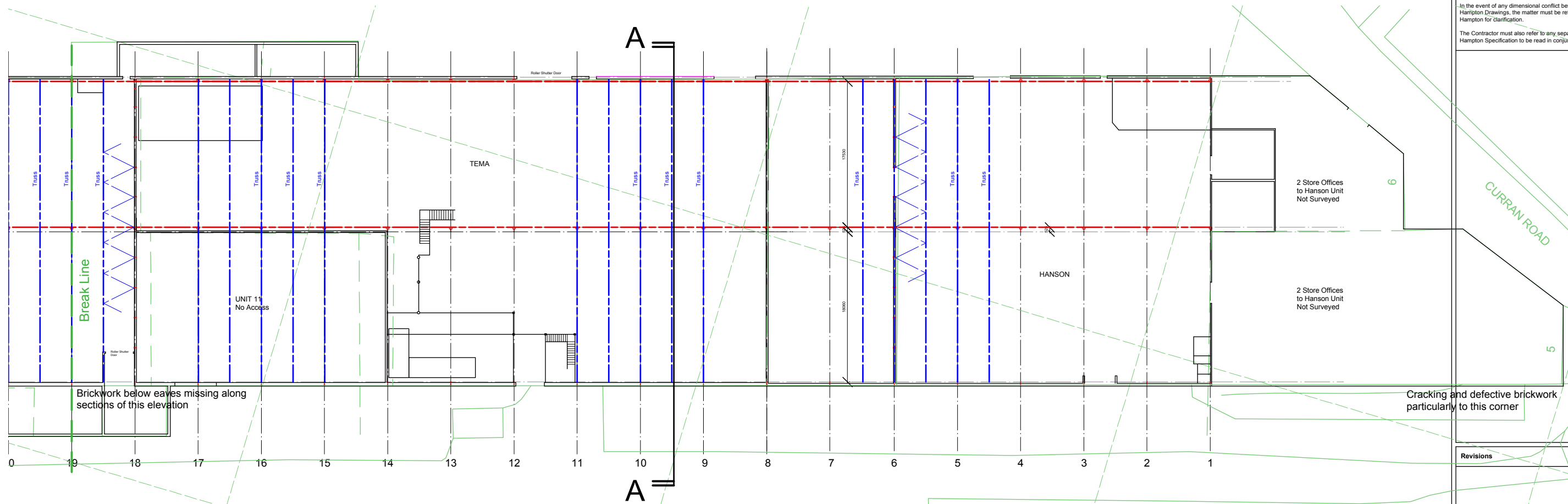
RUAN BRACING
 TO BUILDING
 WALL WIND
 POSTS

NEW GABLE
 FORMED ON
 GL 18.

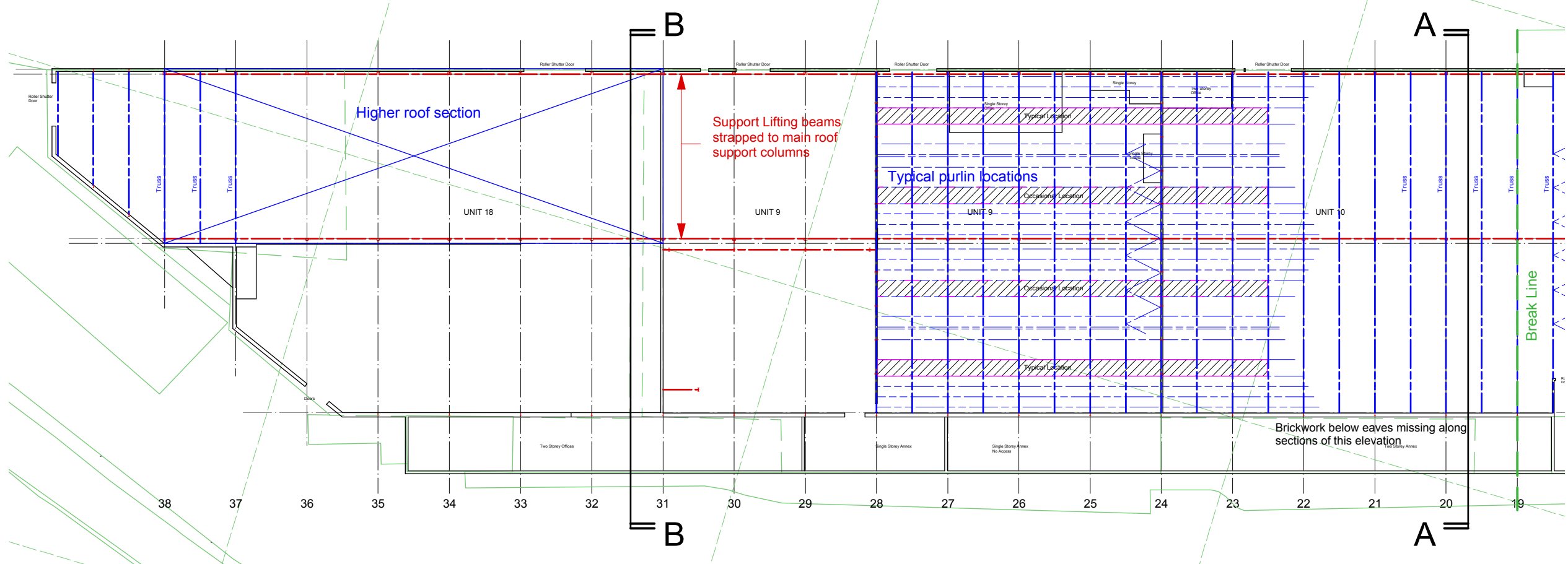
EXTENT OF PROPOSED DEMOLITION

NEW GABLE
 FORMED ON GL 31.

BLOCKWORK WALL WITH
 STEEL FRAMEWORK ABOVE
 SUPPORTING RAILS &
 PROFILED STEEL SHEET
 CLADDING



East Side Plan 1:500 @ A3



West Side Plan 1:500 @ A3

Notes

Where dimensions are not given, drawings must not be scaled and the matter referred to Lambert Smith Hampton.

In the event of any dimensional conflict between Lambert Smith Hampton Drawings, the matter must be referred to Lambert Smith Hampton for clarification.

The Contractor must also refer to any separate Lambert Smith Hampton Specification to be read in conjunction with this drawing.

Revisions



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 Fax: 02920 454235
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Client
 Vastint

Project
 Curran Road Cardiff

Drawing
 Existing Plan

Job No.	DWG No.		
B1664	01		
Scale	Date	Drawn	Checked
1:500 @ A3	Oct 17	ABN	ST

Appendix 5: Bibliography

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**Building Survey & Photographic
Record (Level 3)**

Photographic Record (Volume 2)

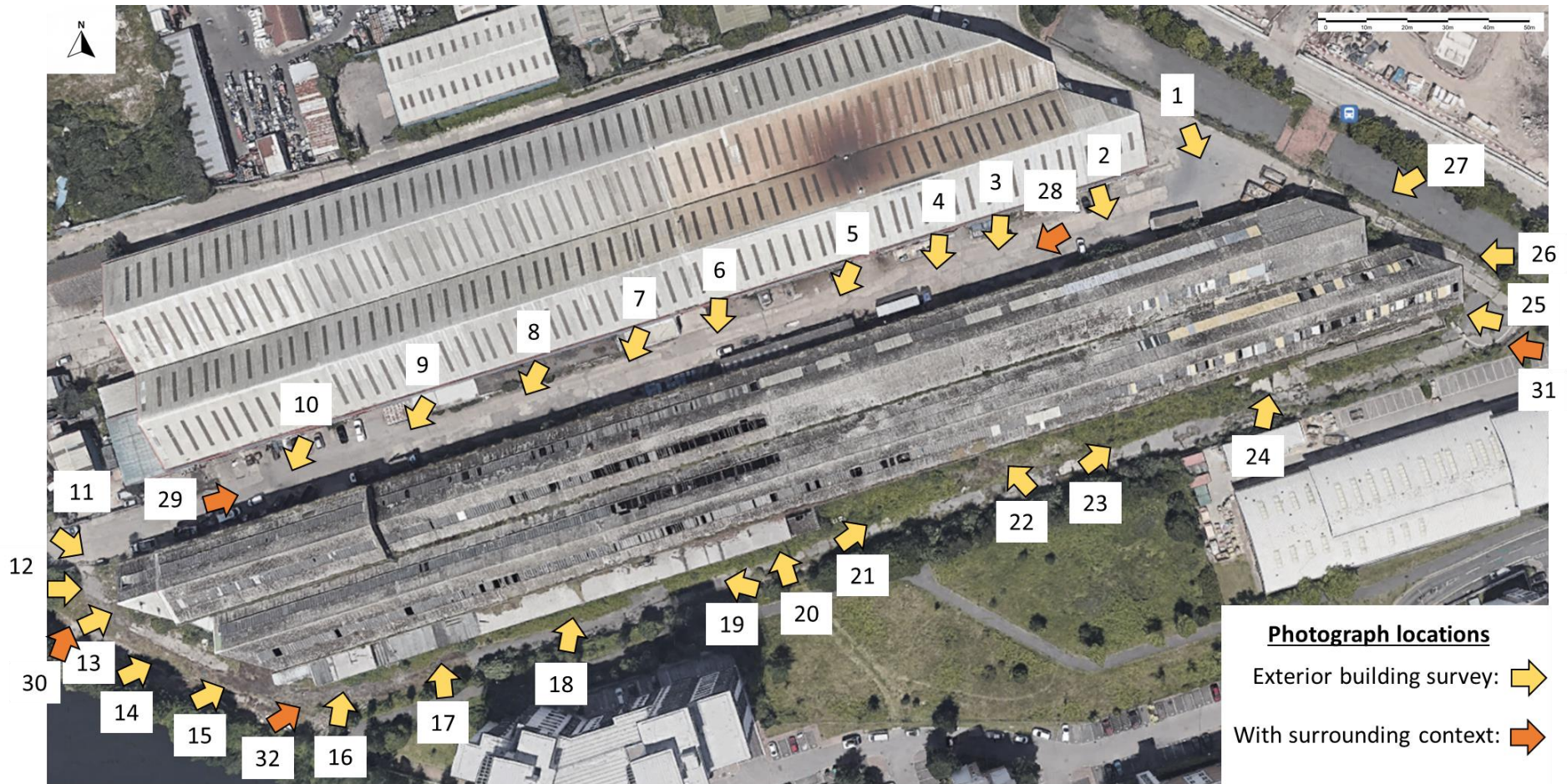
DR017/Orion 5 Building,
Land at Curran Embankment, Cardiff

October 2023

1. Introduction

- 1.1 This Photographic Record (Volume 2) forms part of a Building Recording for the DR017/Orion 5 Building, Land at Curran Embankment, Cardiff, and has been prepared by Turley Heritage. This forms part of a Level 3 Building Recording, with a Written Record (Volume 1) accompanying this Photographic Record (Volume 2).
- 1.2 The photography was undertaken on 9th August 2023 at 12pm, with the north elevations photographed first, followed by the west, south, and east elevations. The recording conditions were generally clear.
- 1.3 The detailed contents of the Photographic Record are set out below:
 - General views of the building within its immediate context
 - The building's external appearance, covering its external elevations indicating the impression of its size, shape and relationship to surrounding buildings and setting
- 1.4 External photographs are provided, with the photographs linked back to an accompanying diagram, with appropriate referencing and numbering to identify the approximate location of the photographs taken.
- 1.5 Internal access and photography were not possible given the high safety risk imposed by the building's poor structural condition, state of repair and asbestos contamination throughout.

2. Photography Location Plan



3. External Photographic Survey



Viewpoint:	1
Details:	North elevation of red brick, large, metal framed multi-paned windows (some covered over with recent metal panels), concrete lintels and sills, corrugated metal roof, formal tripartite entrance with white painted concrete architraves

Viewpoint:	2
Details:	North elevation of red brick, metal framed multi-paned windows, (some covered over with recent metal panels), concrete lintels and sills, corrugated metal roof



Viewpoint:	3
Details:	North elevation of red brick, metal framed multi-paned windows, (some covered over with recent metal panels), concrete lintels and sills, vehicle entrance with steel frame and grey brick infill

Viewpoint:	4
Details:	North elevation of red brick, large, blocked window openings, concrete lintels and sills, metal safety railings at the roofline, vehicle entrance with steel frame



Viewpoint:	5
Details:	North elevation of red brick, metal framed multi-paned windows, (some covered over with recent metal panels), concrete lintels and sills, corrugated roof, single storey projection with steel bars over fenestration

Viewpoint:	6
Details:	North elevation of red brick, metal framed multi-paned windows, (some covered over with recent metal panels), concrete lintels and sills, corrugated roof, windows infilled with breeze blocks, steel framed vehicle entrance



Viewpoint:	7
Details:	North elevation of red brick, concrete lintels and sills, later raised corrugated metal roof structure behind, windows filled with breeze blocks, steel frame vehicle entrance

Viewpoint:	8
Details:	North elevation of red brick, concrete lintels and sills, raised corrugated metal roof structure, steel framed vehicle entrance, windows filled with breeze blocks



Viewpoint:	9
Details:	North elevation of red brick, large industrial windows, concrete lintels and sills, raised corrugated metal roof structure, steel frame vehicle entrance, windows filled with breeze blocks

Viewpoint:	10
Details:	North elevation of red brick, concrete lintels and sills, corrugated metal roof structure, windows filled with breeze blocks



Viewpoint:	11
Details:	North and west elevations of red brick, metal framed multi-paned windows, concrete lintels and sills, corrugated roof, some window openings filled with breeze blocks, corrugated metal roof structure behind

Viewpoint:	12
Details:	West elevation of red brick, metal framed multi-paned windows, concrete lintels and sills, some windows filled with breeze blocks, steel framed vehicle entrance



Viewpoint:	13
Details:	West elevations of red brick, angular roof lines, metal framed multi-paned windows, (some covered over with recent metal panels), concrete lintels and sills, some windows filled with brick and breeze blocks, corrugated metal roof structure behind

Viewpoint:	14
Details:	West elevations of red brick with pitched angular rooflines, concrete lintels and sills, windows filled with brick and breeze blocks, raised corrugated metal roof structure behind



Viewpoint:	15
Details:	West elevation of red brick, concrete lintels and sills, windows filled with breeze blocks, later corrugated metal extension.

Viewpoint:	16
Details:	South elevations of painted red brick, casement windows, corrugated metal extension



Viewpoint:	17
Details:	South elevations of painted and red brick, casement windows, metal framed multi-paned windows

Viewpoint:	18
Details:	South elevation of red brick, metal framed multi-paned windows, concrete lintels and sills, windows boarded up



Viewpoint:	19
Details:	South elevation of red brick, metal framed multi-paned windows, stone lintels and sills, corrugated roof, some windows boarded up

Viewpoint:	20
Details:	South elevation of red brick, metal framed multi-paned windows, casement windows with wire mesh, concrete lintels and sills, windows boarded up



Viewpoint:	21
Details:	South elevation of red bricks, concrete lintels and sills, entrances with metal doors, windows boarded up

Viewpoint:	22
Details:	South elevation of red brick, metal framed multi-paned windows (some covered with metal panels), concrete lintels and sills, metal safety railings



Viewpoint:	23
Details:	South elevation of red brick, metal framed multi-paned windows, concrete lintels and sills, metal safety railings at roofline

Viewpoint:	24
Details:	South elevation of red brick, metal framed multi-paned windows, concrete lintels and sills, vehicle ramp and entrances



Viewpoint:	25
Details:	South and east elevations of red brick with angular roof pitch and definition to Curran Road, metal framed multi-paned windows, concrete lintels and sills, corrugated roof, vehicle ramp and entrance, metal safety railings at roofline

Viewpoint:	26
Details:	East elevations of red brick with angular roof pitch and definition to Curran Road, metal framed multi-paned windows, concrete lintels and sills, pedestrian entrances with wooden doors



Viewpoint:	27
Details:	East elevations of red brick with angular roof pitch and corner to Curran Road, metal framed multi-paned windows, concrete lintels and sills, steel framed vehicle entrance

Viewpoint:	28
Details:	South elevation of subject building of red brick (left) and neighbouring modern warehouse (right)



Viewpoint:	29
Details:	South elevation of subject building of red brick (right) with adjacent modern warehouse (left) and residential blocks under construction beyond Dumballs Road (centre background)

Viewpoint:	30
Details:	West elevations of subject building of red brick with modern adjacent warehouses to the north behind



Viewpoint:	31
Details:	North and east elevations of subject building in red brick with modern tower block at Overtone Court (left) and residential blocks under construction beyond Dumballs Road (right)

Viewpoint:	32
Details:	North elevation of subject building in painted and red brick with modern tower blocks on Loudoun Square to the east (right background)

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